

DEFINITIVE SITE PLAN

FOR

216 & 221 CHANDLER STREET

WORCESTER, MASSACHUSETTS 01610

RECORD APPLICANT:
 POLAR VIEWS, LLC
 89 WEST MAIN STREET UNIT 101
 NORTHBOROUGH, MA 01532

RECORD OWNER:
216 CHANDLER STREET
 DANIEL YARNIE
 89 WEST MAIN STREET UNIT 101
 NORTHBOROUGH, MA 01532

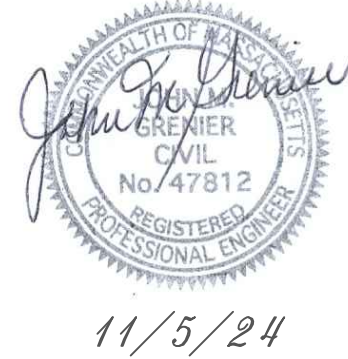
221 CHANDLER STREET
 POLAR VIEWS, LLC
 89 WEST MAIN STREET UNIT 101
 NORTHBOROUGH, MA 01532

LAND PLANNERS—CIVIL ENGINEERS:
 J.M. GRENIER ASSOCIATES INC.
 118 TURNPIKE ROAD SUITE 200
 SOUTHBOROUGH, MA 01772
 (508) 845-2500

LAND SURVEYORS:
 GEO/NETWORK LAND SURVEY INC.
 645 CHANDLER STREET SUITE 7
 WORCESTER, MA 01610

ZONING DISTRICT: BUSINESS, GENERAL (BG-3.0)
 OVERLAY DISTRICT: COMMERCIAL CORRIDORS (CCOD-E)

INDEX DESCRIPTION	SHEET NUMBER
COVER	1 OF 10
EXISTING CONDITIONS PLANS	2-3 OF 10
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REV. NO.	DATE	REVISION
SCALE: AS SHOWN		DATE: NOVEMBER 5, 2024
COVER SHEET		
SHEET NO.: SHEET 1 OF 10		PROJECT NO.: G-698

FOR REGISTRY OF DEEDS USE ONLY

PLAN REFERENCES

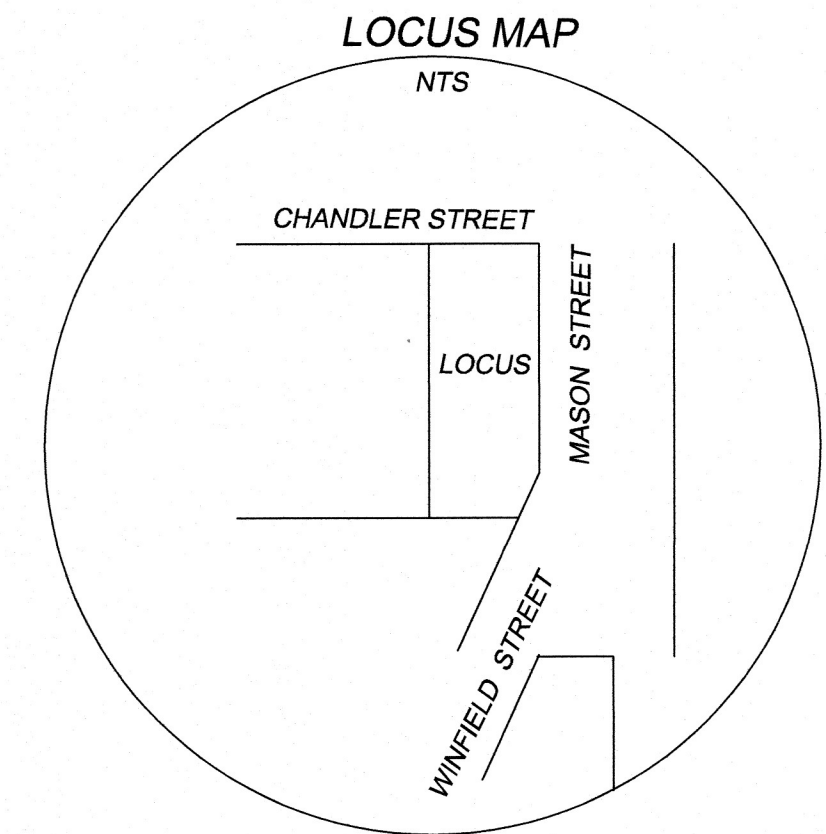
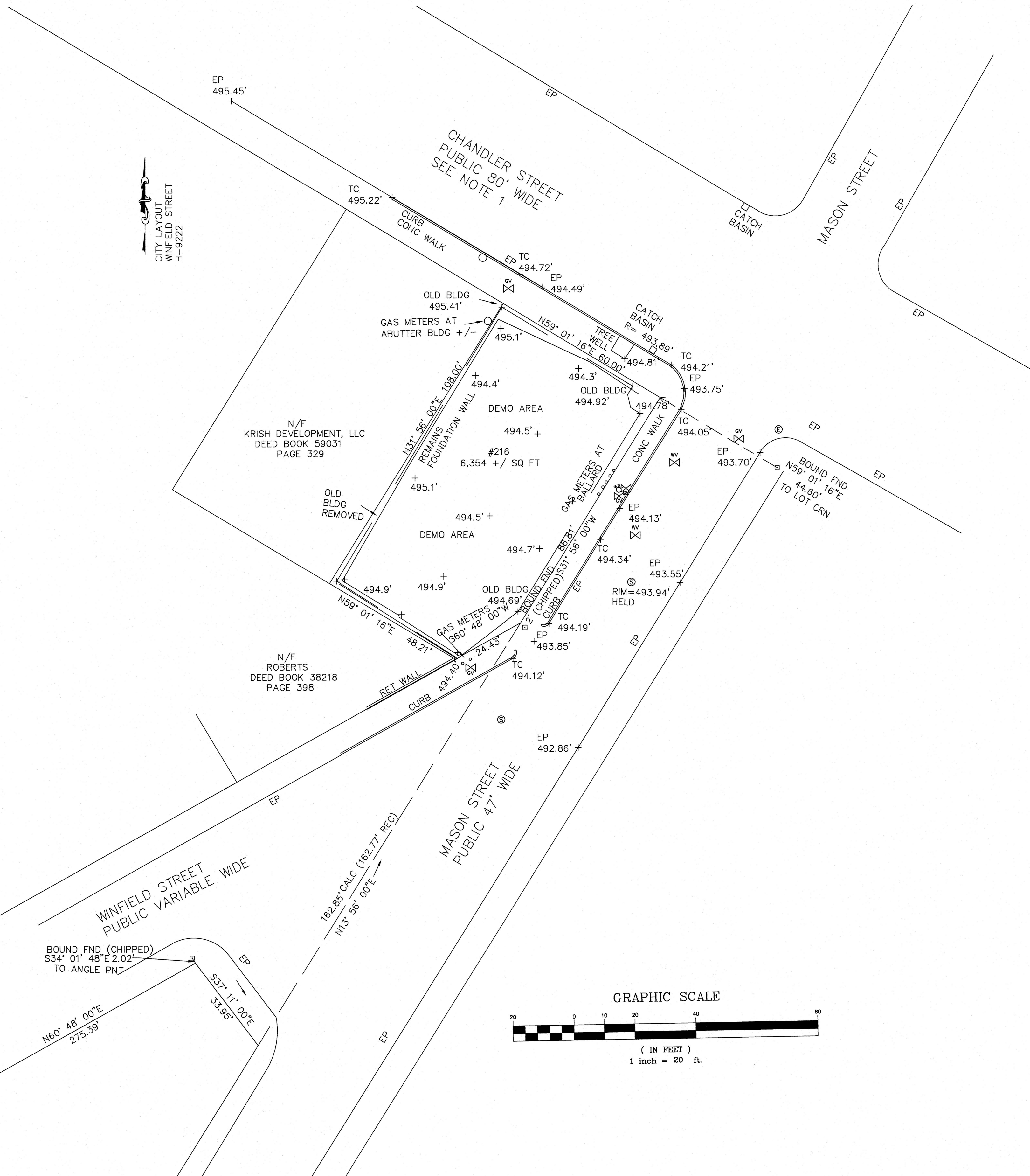
PLAN BOOK 779 PLAN 8
DEED BOOK 478 PLAN 653

CITY OF WORCESTER PLANS
CHANDLER STREET (H-9236-6)
DEWEY STREET (H-9340-4)
MASON STREET (H-9323)
WINFIELD STREET (H-9222)

MGL C41 S81X SURVEYOR'S CERTIFICATE:

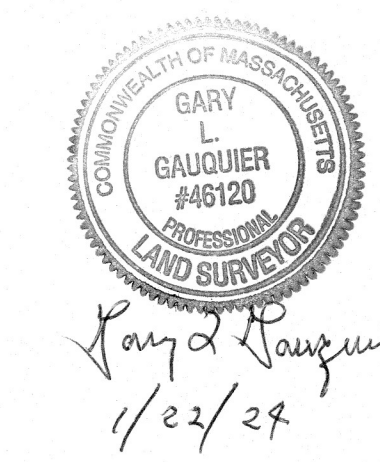
I HEREBY CERTIFY THAT THE PROPERTY LINES SHOWN ON THIS PLAN ARE THE LINES DIVIDING EXISTING OWNERSHIPS, AND THE LINES OF THE STREETS AND WAYS SHOWN ARE THOSE OF PUBLIC OR PRIVATE STREETS OR WAYS ALREADY ESTABLISHED AND THAT NO NEW LINES FOR DIVISION OF EXISTING OWNERSHIP OR FOR NEW WAYS ARE SHOWN.

1/23/24 DATE
Gary L. Gauguier SURVEYOR



NOTES

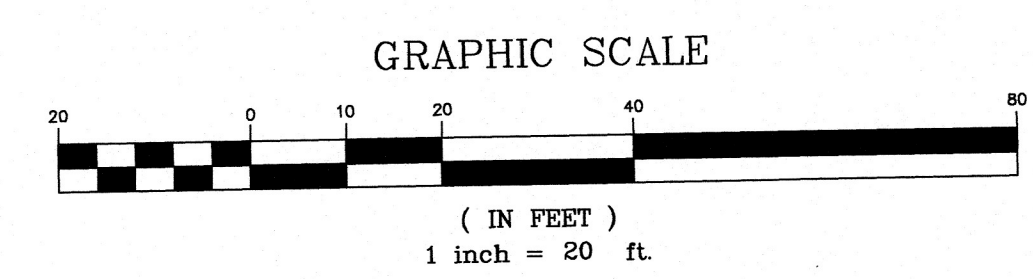
- 1.) UTILITIES AT CHANDLER STREET NOT LOCATED
- 2.) DATUM TAKEN FROM SMH ON UTILITY PLAN FROM CITY OF WORCESTER
- 3.) CONSTRUCTION ON LOTS OR LAND IS SUBJECT TO ANY EASEMENTS, RIGHT OF WAYS, RESTRICTION RESERVATIONS OR LIMITATIONS ON RECORD
- 4.) UTILITIES SHOWN ARE FROM FIELD LOCATIONS IT SHALL BE THE RESPONSIBILITY OF THE DESIGN ENGINEER AND THE CONTRACTOR TO VERIFY THE SIZE, ELEVATION AND LOCATION, AND TO CONTACT "DIG-SAFE AT LEAST 72 HOURS PRIOR TO ANY EXCAVATION, DEMOLITION OR CONSTRUCTION



OWNER OF RECORD
CHANDLER MASON REALTY TRUST
DEED BOOK 35862 PAGE 345

REVISIONS

REV	DESC	REV_BY	DATE
1	SPOT SHOTS	GLG	1/22/24



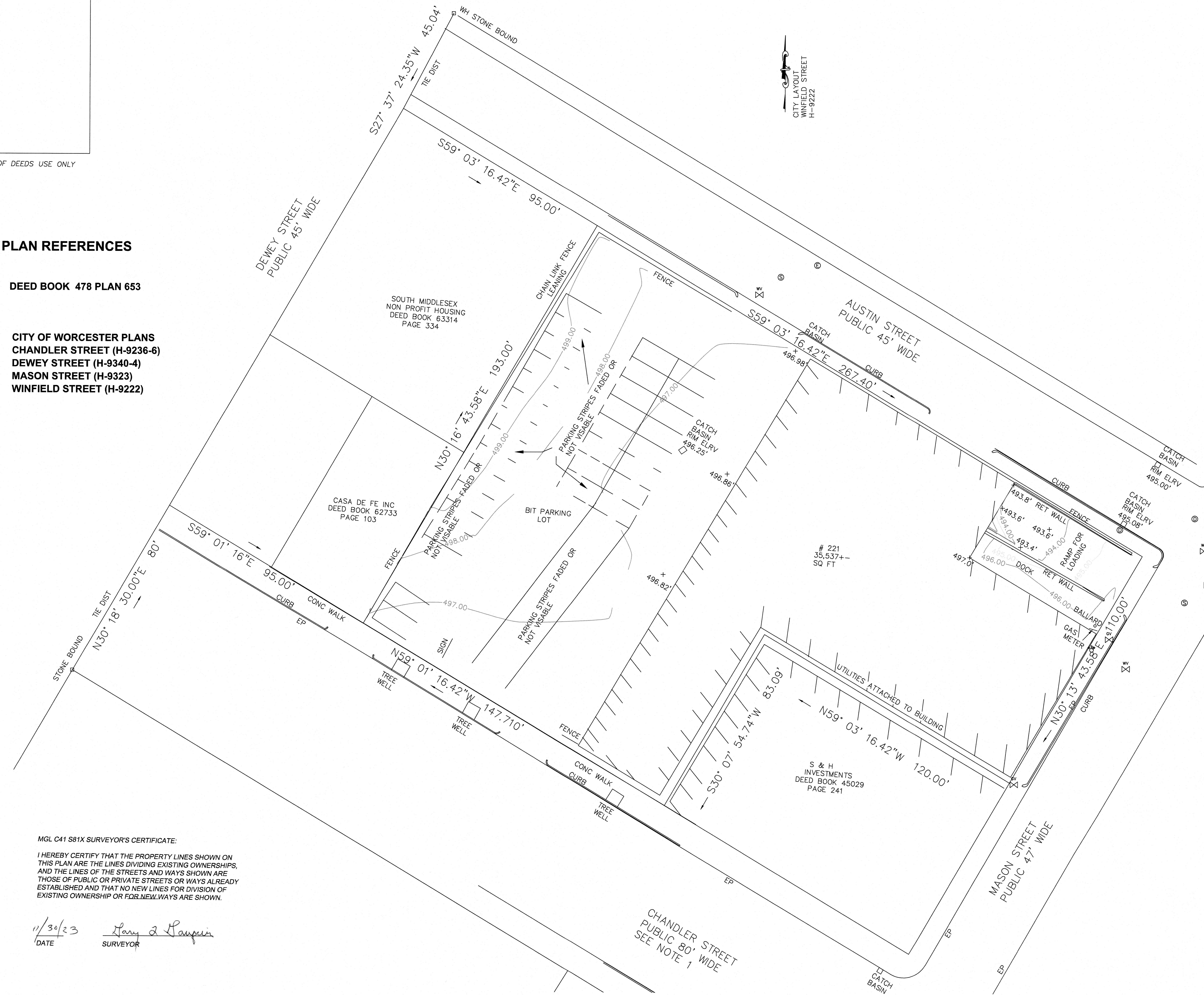
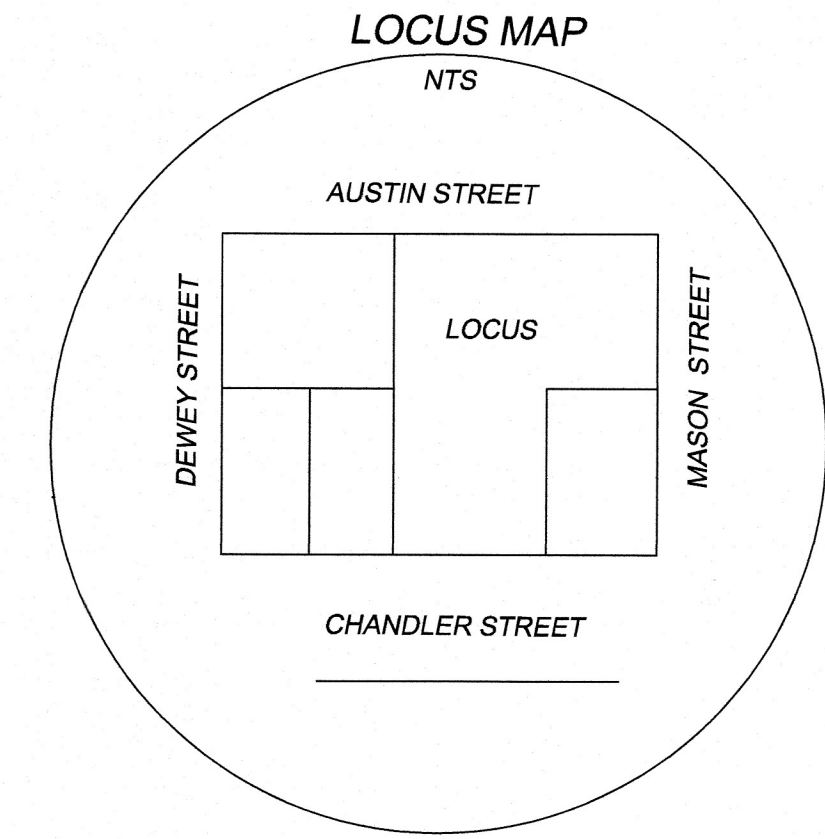
PLAN OF LAND
216 CHANDLER STREET
WORCESTER, MA
PREPARED FOR: DANIEL AND REBECCA YARNIE
MARCH 29, 2022 SCALE 1" = 20'
GEO / NETWORK LAND SURVEY, INC.
645 CHANDLER STREET SUITE 7
WORCESTER, MASSACHUSETTS 01610
508-755-7003 FAX 508-755-8003

FOR REGISTRY OF DEEDS USE ONLY

PLAN REFERENCES

DEED BOOK 478 PLAN 653

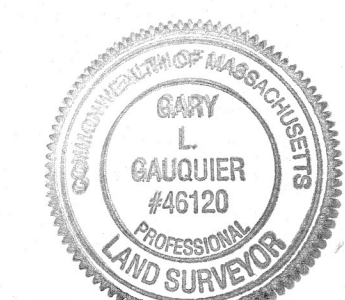
CITY OF WORCESTER PLANS
CHANDLER STREET (H-9236-6)
DEWEY STREET (H-9340-4)
MASON STREET (H-9323)
WINFIELD STREET (H-9222)



NOTES

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- 2.) DATUM TAKEN FROM SMH ON UTILITY PLAN FROM CITY OF WORCESTER
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OWNER OF RECORD
POLAR VIEWS
DEED BOOK 68988 PAGE 48



Cary L. Gauquier
11/30/23

PLAN OF LAND
216 CHANDLER STREET
WORCESTER, MA

PREPARED FOR: POLAR VIEWS
NOVEMBER 30, 2023 SCALE 1" = 20'

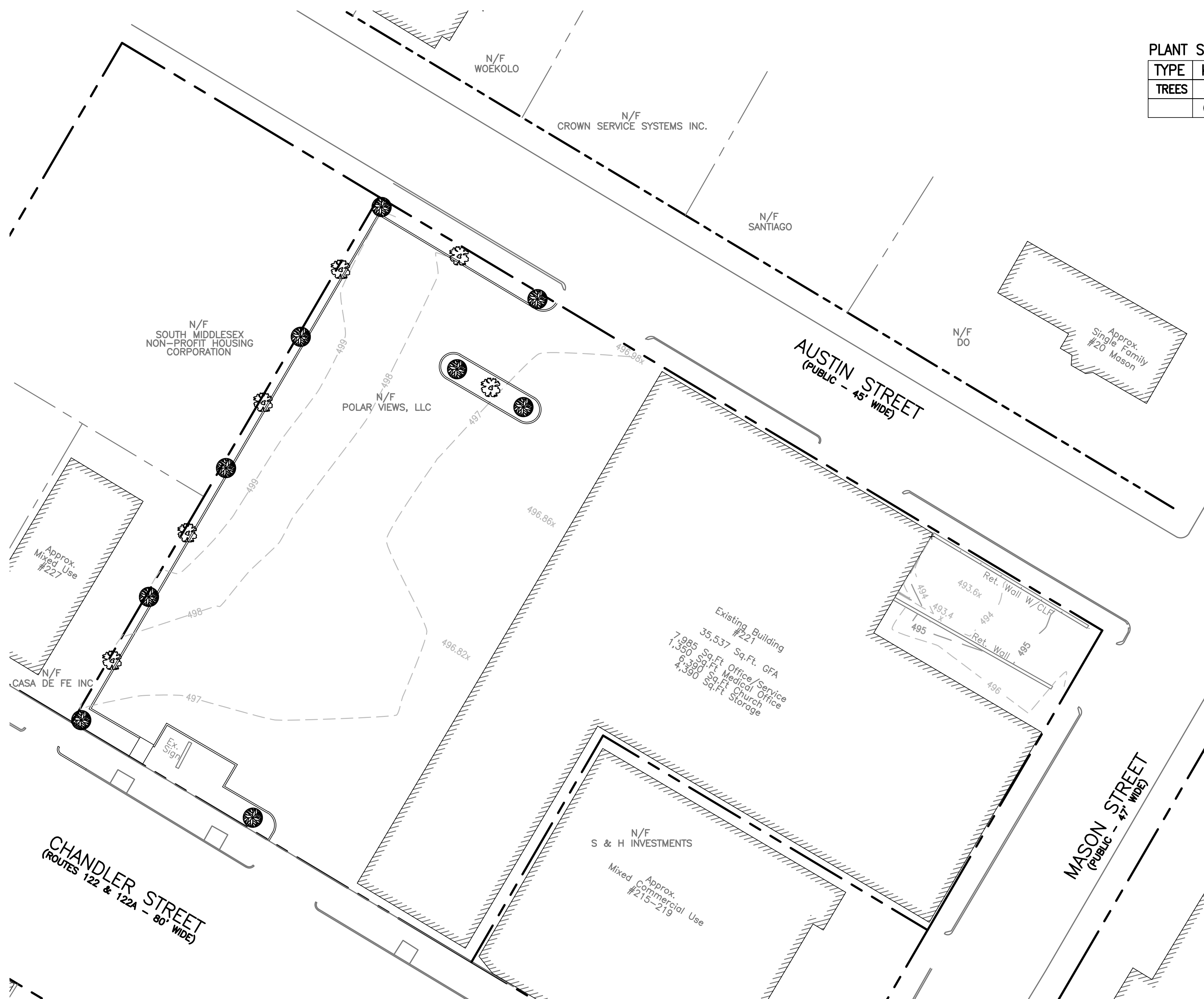
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508-755-7003 FAX 508-755-8003

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11/30/23 DATE *Cary L. Gauquier* SURVEYOR

CHANDLER STREET PUBLIC 80' WIDE
SEE NOTE 1



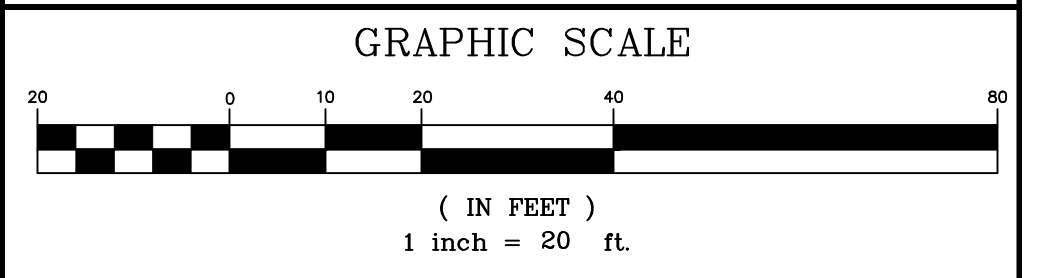
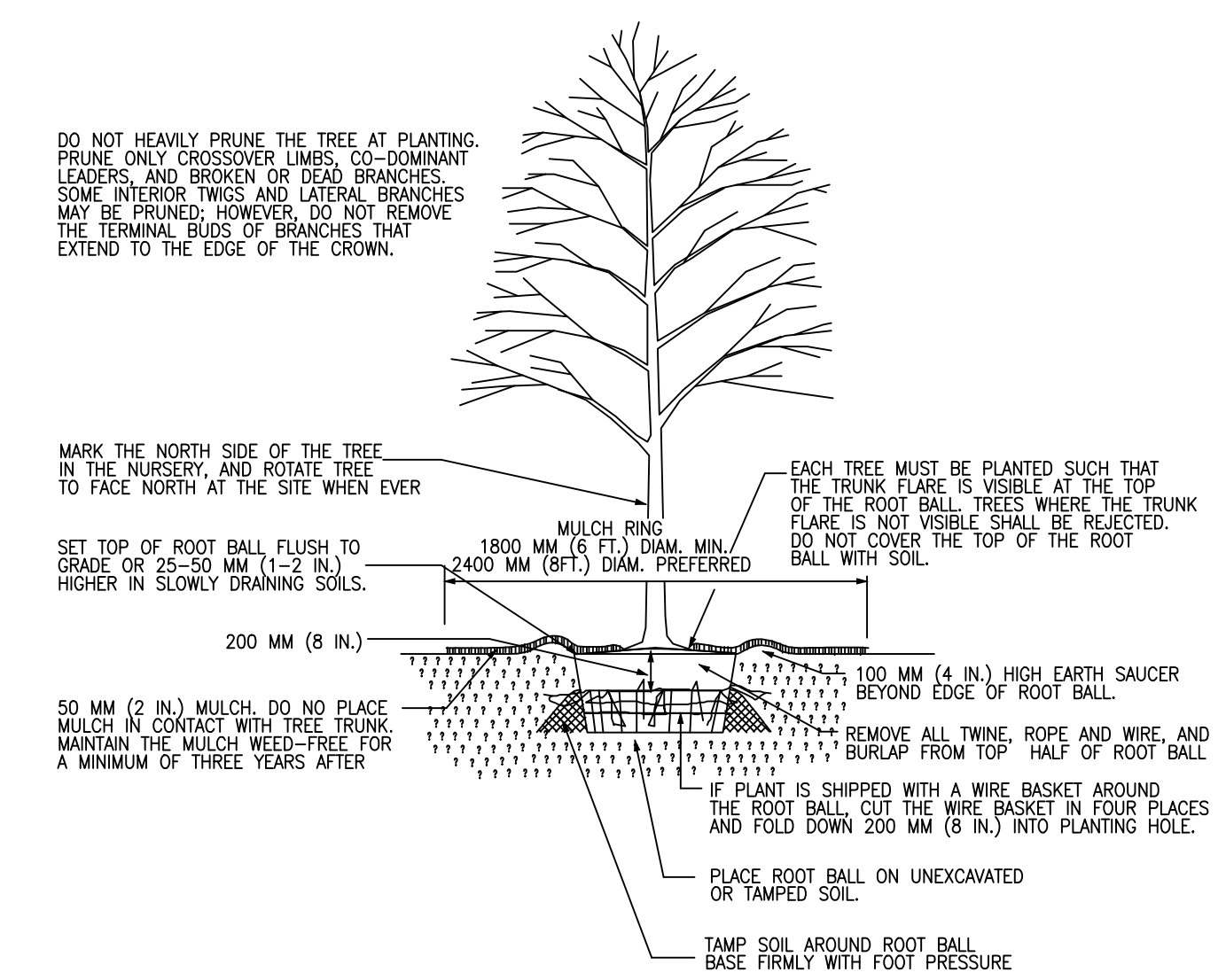
PLANT SCHEDULE

TYPE	KEY	BOTANICAL NAME	COMMON NAME	SIZE	CONDITION	QTY
TREES		<i>Quercus palustris</i>	Pin Oak	3 1/2" cal.	B&B	6
		<i>Cornus kousa</i>	Kousa Dogwood	3 1/2" cal.	B&B	9

LEGEND:

- EXISTING PROPERTY LINE
- EXISTING CONTOUR - HIGH
- EXISTING CONTOUR - LOW
- PROPOSED CONTOUR - HIGH
- PROPOSED CONTOUR - LOW
- EXISTING EDGE PAVEMENT
- EXISTING CURB
- PROPOSED EDGE OF PAVEMENT
- PROPOSED CURB
- EXISTING DRAIN LINE
- PROPOSED DRAIN LINE
- EXISTING WATER LINE
- PROPOSED WATER LINE
- EXISTING SEWER LINE
- PROPOSED SEWER LINE
- PROPOSED EROSION CONTROL

- NOTES:**
- INTERIOR TREES REQUIRES: 1/10 INTERIOR SPACES X 30 SPACES=3 INTERIOR TREES PROVIDED = 3
 - ALL PLANTINGS SHALL BE ASIAN LONGHORNED BEETLE (ALB) AND EMERALD ASH BORER RESISTANT.
 - SNOW STORAGE SHALL NOT BE LOCATED WITHIN ANY REQUIRED PARKING SPACES OR LANDSCAPED BUFFERS AND SHALL NOT IMPEDE VISIBILITY. ONCE DESIGNATED SNOW STORAGE AREAS EXCEED CAPACITY SNOW SHALL BE REMOVED FROM THE SITE.



REV. NO.	DATE	REVISION

TITLE:

**DEFINITIVE SITE PLAN
FOR
216 & 221 CHANDLER STREET
WORCESTER, MASSACHUSETTS 01610**

PREPARED FOR:

**POLAR VIEWS, LLC
89 WEST MAIN STREET UNIT 101
NORTHBOROUGH, MASSACHUSETTS 01532**

PROPERTY OWNER:

**DANIEL YARNIE
89 WEST MAIN STREET UNIT 101
NORTHBOROUGH, MASSACHUSETTS 01532**

PREPARED BY:

**J.M. GRENIER ASSOCIATES INC.
118 TURNPIKE ROAD SUITE 200
SOUTHBOROUGH, MASSACHUSETTS 01772**

TELE. NO.: (508) 845-2500 jmgrenier@townisp.com

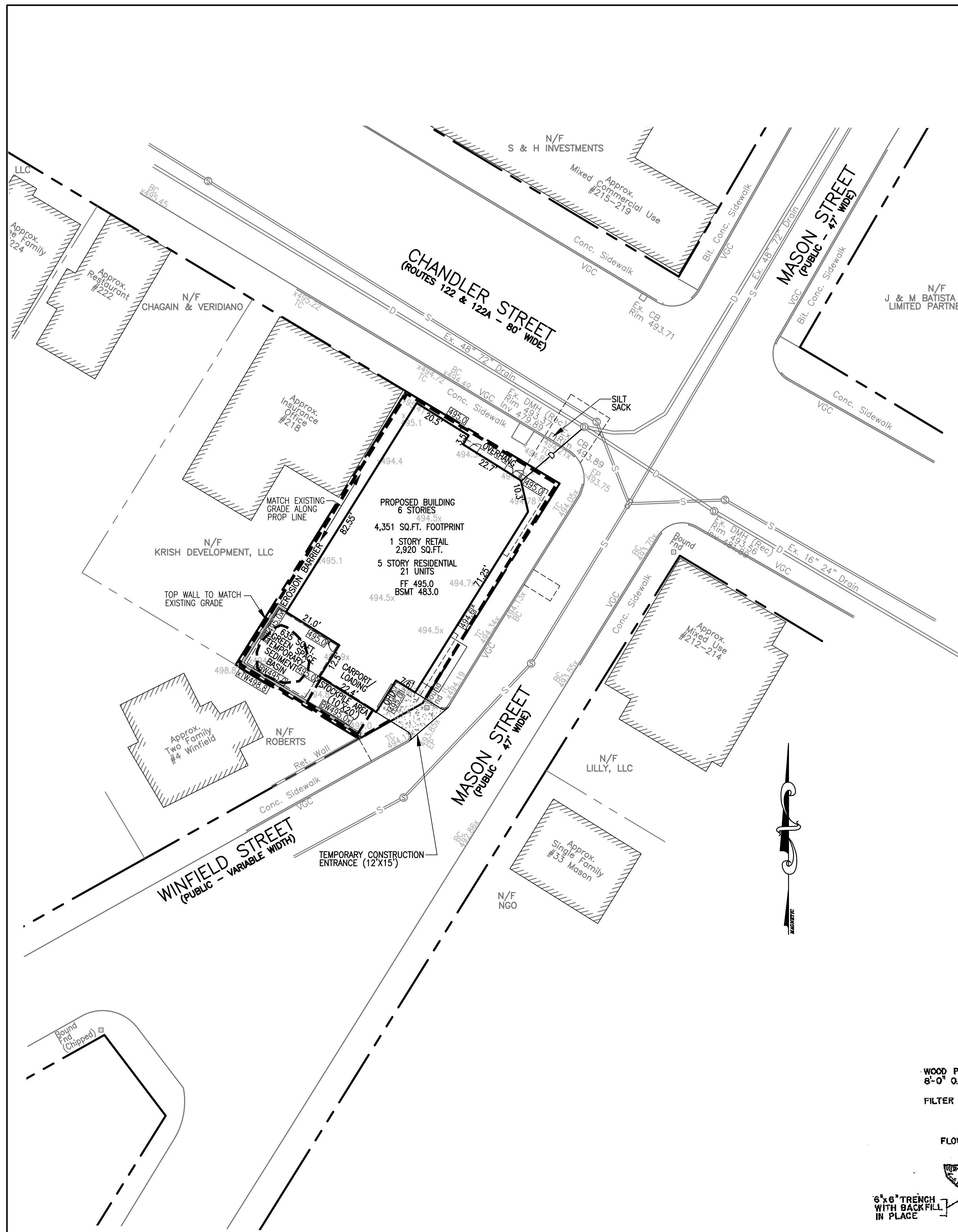
SCALE: 1" = 20' **DATE:** NOVEMBER 5, 2024

LANDSCAPE PLAN

SHEET NO.: SHEET 8 OF 10 **PROJECT NO.:** G-698



11/5/24



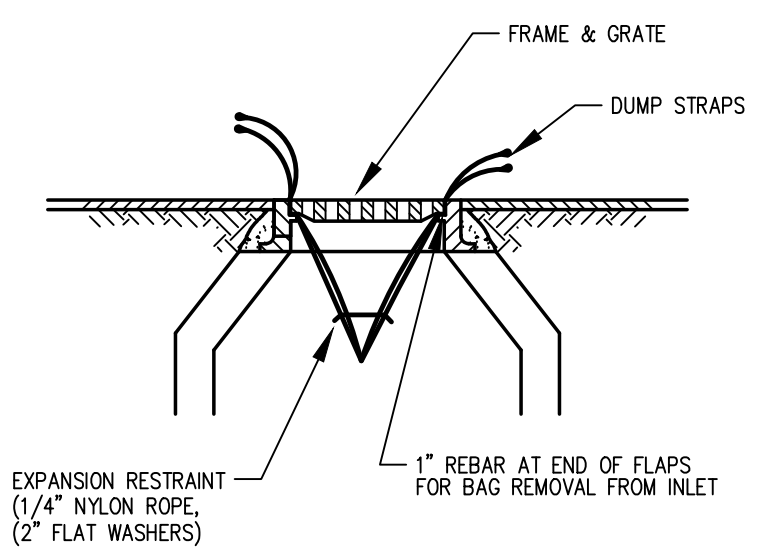
1. Seeding and repairs shall be performed as required. Sediment and debris shall be removed at least once a year, typically in early spring prior to the commencement of the growing season.

2. The catch basin shall be inspected annually. Unit shall be cleaned when accumulated sediments reach a depth of 6 inches. Accumulated sediment must be disposed of in accordance with applicable local state, and federal guidelines and regulations. The contractor will be responsible for the maintenance of the unit until such time as the site work is complete. The maintenance will then be the responsibility of the owner(s).

3. The infiltration chambers shall be inspected after every major storm for the first 3 months to ensure proper stabilization and function. The chambers shall be inspected once per year after that. Water levels should be inspected and recorded for several days after a major storm event to track infiltration capability.

4. The contractor will be responsible for the maintenance of all drainage structures and until such time as the site work is complete. The maintenance will then be the responsibility of the property owners.

STORMWATER COLLECTION & TREATMENT SYSTEM INSPECTION & MAINTENANCE GUIDELINES



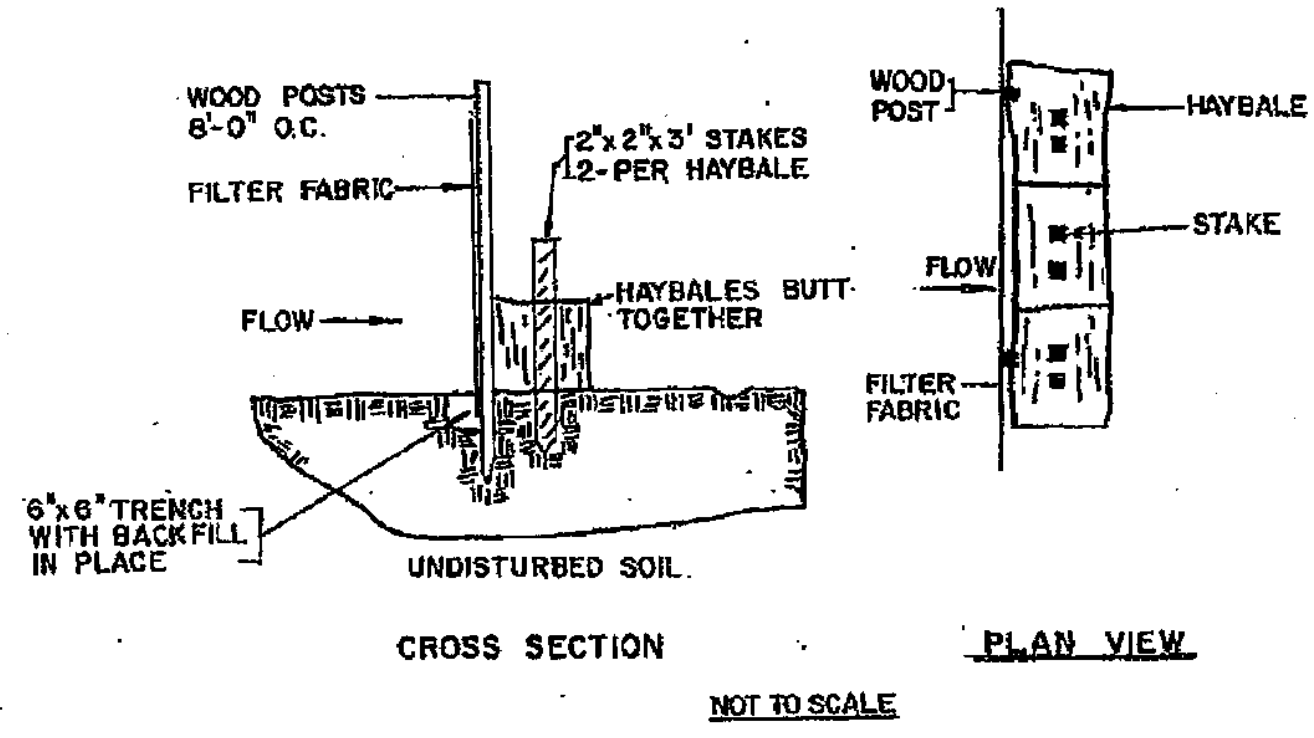
NOTE
BAGS SHOULD BE CLEANED OUT AFTER EVERY RAIN EVENT AND/OR AS NEEDED.

SILT SACK DETAIL
NOT TO SCALE

CONSTRUCTION SEQUENCING:

- SELECTIVELY CUT TREES AND CLEAR BRUSH TO BE CHIPPED AND HAULED OFF SITE. NOTE THAT THE SITE IS IN THE ASIAN LONGHORNED BEETLE (ALB) REGULATED AREA.
- STAKE LOCATION OF AND INSTALL EROSION CONTROL BARRIER, CONSTRUCTION ENTRANCE AS DELINEATED ON SITE PLAN.
- STRIP TOP AND SUBSOIL AS NECESSARY IN WORK AREA. STOCKPILE MATERIAL ON SOUTHERN PORTION OF LOT FOR BACKFILLING PURPOSES AT COMPLETION OF FOUNDATION CONSTRUCTION.
- FORM AND POUR FOUNDATION FOR NEW BUILDING.
- BACKFILL FOUNDATION AREAS AS NECESSARY
- CONSTRUCT BUILDING.
- CONSTRUCT DRIVEWAY AND LOAM AND SEED DISTURBED AREAS.
- ALL EXCESS MATERIALS TO BE REMOVED FROM THE SITE AS SOON AS PRACTICAL.
- ALL GRADED AREAS TO BE LANDSCAPED, LOAMED AND SEEDED AS SOON AS PRACTICAL TO REDUCE ANY POTENTIAL EROSION.

NOTE: PROCESSES AS DESCRIBED ABOVE ARE ESTIMATED TO TAKE 9 MONTHS. WINTER CONDITIONS MAY AFFECT THIS SCHEDULE.



GENERAL:

- THIS PLAN IS INTENDED TO PROVIDE GUIDANCE AND INSTRUCTION TO THE CITY OF WORCESTER AND THE CONTRACTOR(S) IN THE PREVENTION OF EROSION AND SEDIMENTATION OFF SITE. THIS PLAN IS INTENDED TO ALLOW ANY CONDITIONS OF APPROVAL TO BE MORE SPECIFIC IN ADDRESSING ITEMS OF CONCERN. IF CONDITIONS BY THE CITY OF WORCESTER ARE IN CONFLICT WITH REQUIREMENTS AS SPECIFIED ON THE PLANS CONDITIONS AS SPECIFIED BY THE CITY OF WORCESTER SHALL SUPERSEDE THE REQUIREMENTS SPECIFIED ON THESE PLANS.

PRE-CONSTRUCTION:

- AN EROSION CONTROL BARRIER (SEE BELOW) SHALL BE INSTALLED AS DEPICTED ON THE PLANS, BETWEEN THE AREAS TO BE DISTURBED AND RECEIVING DRAINAGE WAY AND STRUCTURES. THIS BARRIER SHALL REMAIN IN PLACE UNTIL ALL TRIBUTARY SURFACES HAVE BEEN FULLY STABILIZED. THE EROSION CONTROL BARRIERS AS SHOWN ON THE PLANS ARE THE MINIMUM PROTECTIVE MEASURES REQUIRED TO PROTECT THE ON AND OFF SITE DRAINAGE SYSTEMS.
- THE CONTRACTOR SHALL ESTABLISH A STAGING AREA AS SHOWN ON THE PLAN FOR THE OVERNIGHT STORAGE OF EQUIPMENT AND STOCKPILING OF MATERIALS.
- IN THE STAGING AREA, THE CONTRACTOR SHALL HAVE A STOCKPILE OF MATERIALS REQUIRED TO CONTROL EROSION ON-SITE TO BE USED TO SUPPLEMENT OR REPAIR EROSION CONTROL DEVICES. THESE MATERIALS SHALL INCLUDE, BUT ARE NOT LIMITED TO: HAY BALES, SILT FENCE AND CRUSHED STONE.
- A TEMPORARY STONE CONSTRUCTION ENTRANCE IS REQUIRED TO PREVENT TRACKING OF SILT, MUD, ETC. ONTO EXISTING ROADS. THE STONE SHALL BE REPLACED REGULARLY AS WELL AS WHEN THE STONE IS SILT LOADED OR EQUIPMENT IS OBSERVED TO BE TRACKING SOIL ONTO THE ROADWAYS. SEE GENERAL CONSTRUCTION REQUIREMENTS, SEE NOTE #4 BELOW.
- THE CONTRACTOR IS RESPONSIBLE FOR EROSION CONTROL ON & OFF SITE AND SHALL UTILIZE EROSION CONTROL MEASURES WHERE NEEDED, REGARDLESS OF WHETHER THE MEASURES ARE SPECIFIED HEREIN, ON THE PLAN OR IN ANY ORDER OF CONDITIONS.

PRELIMINARY SITE WORK:

- MATERIAL REMOVED SHOULD BE STOCKPILED, SEPARATING THE TOPSOIL FOR FUTURE USE ON THE SITE OR IN A SECURED OFF SITE AREA APPROVED BY THE OWNER. EROSION CONTROLS SHALL BE UTILIZED ALONG THE DOWN SLOPE OF THE PILES IF THE PILES ARE TO REMAIN FOR MORE THAN THREE WEEKS.
- IF INTENSE RAINFALL IS ANTICIPATED, THE INSTALLATION OF SUPPLEMENTAL HAY BALE DIKES, SILT FENCES, OR ARMORED DIKES SHALL BE UTILIZED.
- IF THE SITE CONSTRUCTION OCCURS AT ANY TIME OTHER THAN THE APRIL - DECEMBER CONSTRUCTION SEASON, ALL DRAINAGE SYSTEMS TEMPORARY OR PERMANENT SHALL MAINTAIN TO ENSURE ADEQUATE HYDRAULIC CAPACITY, AND DRAINING CHARACTERISTICS.

EROSION CONTROL MEASURES:

- THE CONTRACTOR SHALL BE RESPONSIBLE FOR ENSURING THAT EROSION AND SEDIMENTATION ARE CONTROLLED. THIS PLAN SHALL BE ADAPTED TO FIT THE CONTRACTOR'S EQUIPMENT, WEATHER CONDITIONS, AND ANY CONDITIONS ISSUED BY THE CITY OF WORCESTER.
- THE MOST IMPORTANT ASPECTS OF CONTROLLING EROSION AND SEDIMENTATION ARE LIMITING THE EXTENT OF DISTURBANCE AND STABILIZING SURFACES AS SOON AS POSSIBLE. OF SECONDARY IMPORTANCE IN EROSION CONTROL IS THE LIMITING THE SIZE AND LENGTH OF THE TRIBUTARY DRAINAGE AREA WITHIN THE WORK SITE AND DRAINAGE STRUCTURES. THESE FUNDAMENTAL PRINCIPLES SHALL BE THE KEY FACTOR IN THE CONTRACTOR'S CONTROL OF EROSION ON THE SITE.
- THE EXISTING SOIL CONDITIONS PROVIDE THE POTENTIAL OF RUNOFF TO OFF-SITE AREAS WITH EROSION POTENTIAL.
- ALL DISTURBED SURFACES SHALL BE STABILIZED A MINIMUM OF 14 DAYS AFTER CONSTRUCTION IN ANY PORTION OF THE SITE HAS CEASED OR IS TEMPORARILY HALTED UNLESS ADDITIONAL CONSTRUCTION IS INTENDED TO BE INITIATED WITHIN 21 DAYS.
- THE CONTRACTOR IS RESPONSIBLE FOR THE MAINTENANCE AND REPAIR OF ALL EROSION CONTROL DEVICES WITHIN THE LIMIT OF WORK. ALL EROSION CONTROL DEVICES SHALL BE REGULARLY INSPECTED. ANY SEDIMENTS REMOVED FROM THE CONTROL DEVICES SHALL BE DISPOSED OF.
- AT NO TIME SHALL SILT-LADEN WATER BE ALLOWED TO ENTER SENSITIVE AREAS (WETLANDS, OFF-SITE AREAS AND DRAINAGE SYSTEMS). ANY RUNOFF FROM DISTURBED SURFACES SHALL BE DIRECTED THROUGH SETTLING BASINS AND EROSION CONTROL BARRIERS PRIOR TO ENTERING ANY SENSITIVE AREAS.

GENERAL CONSTRUCTION REQUIREMENTS:

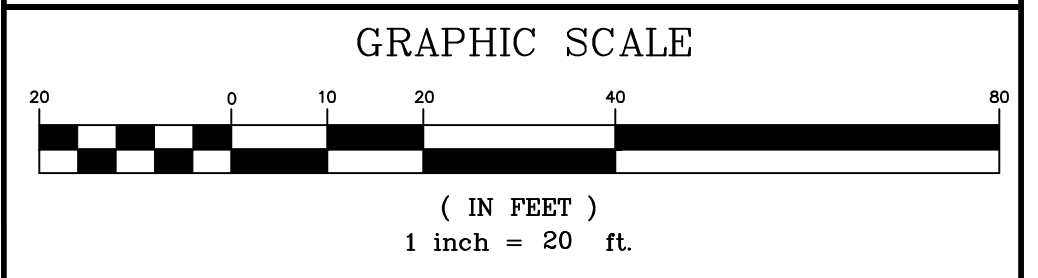
- ANY REFUELING OF CONSTRUCTION VEHICLES AND EQUIPMENT SHALL NOT TAKE PLACE INSIDE OF A 100 FOOT BUFFER ZONE, NEAR THE ENTRANCE TO ANY DRAINAGE SYSTEM AND SHALL NOT BE CONDUCTED IN PROXIMITY TO SEDIMENTATION BASINS.
- NO ON-SITE DISPOSAL OF STUMPS, SOLID WASTE, INCLUDING CONSTRUCTION MATERIALS IS ALLOWED.
- NO MATERIALS SHALL BE DISPOSED OF INTO THE WETLANDS, OR EXISTING/PROPOSED DRAINAGE SYSTEMS. ALL CONTRACTORS INCLUDING: CONCRETE SUPPLIERS, PAINTERS AND PLASTERERS, SHALL BE INFORMED THAT THE CLEANING OF EQUIPMENT IS PROHIBITED IN AREAS WHERE THE WASH-WATER WILL DRAIN DIRECTLY TO THE SITE DRAINAGE SYSTEMS.
- THE CONTRACTOR SHALL UNDERTAKE ALL WORK TO LIMIT AIRBORNE SEDIMENTS, ONLY CLEAN, POTABLE WATER MAY BE USED TO CONTROL DUST.

LANDSCAPING:

- LANDSCAPING SHALL OCCUR AS SOON AS POSSIBLE TO PROVIDE PERMANENT STABILIZATION OF DISTURBED SURFACES.
- CONTRACTOR SHALL UTILIZE A VARIETY OF SLOPE STABILIZATION METHODS AND MATERIALS WHICH SHALL BE ADJUSTED TO THE SITE CONDITIONS. EROSION CONTROL BLANKETS OR MIRAFI MIRAMAT (OR SIMILAR PRODUCTS) SHALL BE AVAILABLE ON SITE.
- IF THE SEASON OR ADVERSE WEATHER CONDITIONS DO NOT ALLOW THE ESTABLISHMENT OF VEGETATION, TEMPORARY MULCHING WITH HAY, TAGGED WOOD CHIPS OR OTHER METHODS SHALL BE PROVIDED.
- ALL DISTURBED SURFACES TO BE PLANTED SHALL RECEIVE A MINIMUM OF 6" TOPSOIL SHALL BE PLACED AND ITS SURFACE SMOOTHED TO THE SPECIFIED GRADES.
- ALL SLOPES OF 2.5:1 OR GREATER SHALL BE STABILIZED WITH EROSION CONTROL FABRIC.
- TO ENSURE A DENSE, SUCCESSION GROWTH, SEED IS REQUIRED ON ALL DISTURBED SURFACES.

LEGEND:

---	EXISTING PROPERTY LINE
-490-	EXISTING CONTOUR - HIGH
-491-	EXISTING CONTOUR - LOW
-490-	PROPOSED CONTOUR - HIGH
-491-	PROPOSED CONTOUR - LOW
---	EXISTING EDGE PAVEMENT
---	EXISTING CURB
---	PROPOSED EDGE OF PAVEMENT
---	PROPOSED CURB
D	EXISTING DRAIN LINE
D	PROPOSED DRAIN LINE
W	EXISTING WATER LINE
W	PROPOSED WATER LINE
S	EXISTING SEWER LINE
S	PROPOSED SEWER LINE
---	PROPOSED EROSION CONTROL



REV. NO.	DATE	REVISION

TITLE:

DEFINITIVE SITE PLAN
FOR
216 & 221 CHANDLER STREET
WORCESTER, MASSACHUSETTS 01610

PREPARED FOR:

POLAR VIEWS, LLC
89 WEST MAIN STREET UNIT 101
NORTHBOROUGH, MASSACHUSETTS 01532

PROPERTY OWNER:

DANIEL YARNIE
89 WEST MAIN STREET UNIT 101
NORTHBOROUGH, MASSACHUSETTS 01532

PREPARED BY:

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TELE NO.: (508) 845-2500 jmgreiner@townisp.com

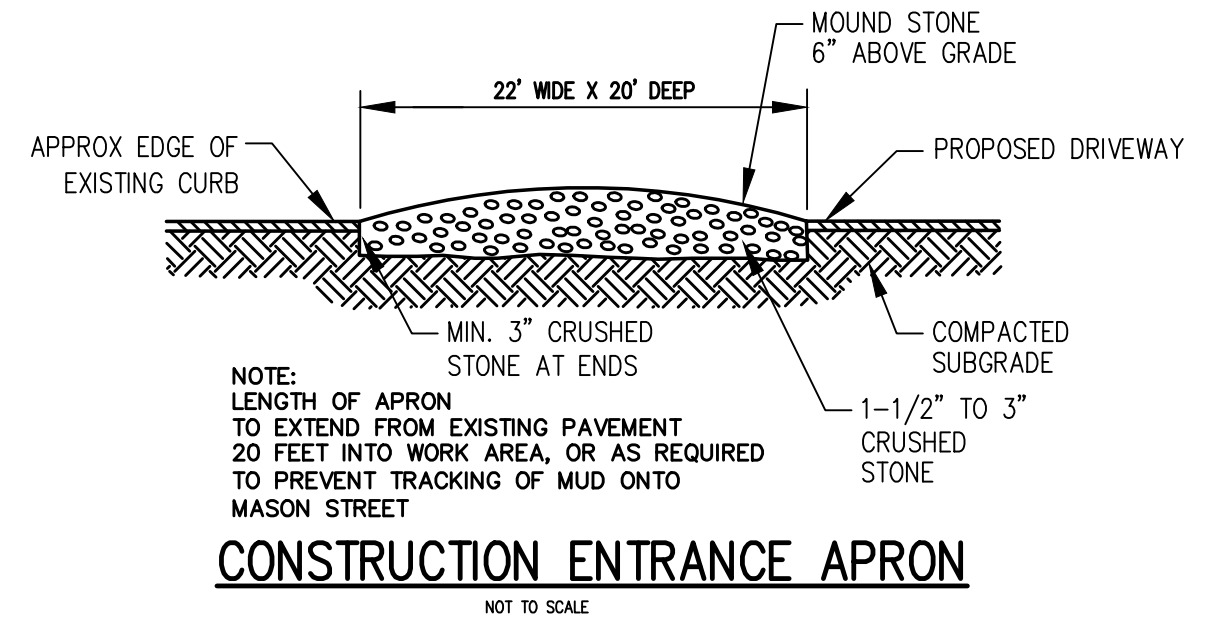
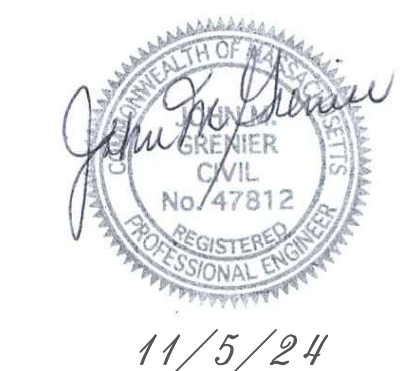
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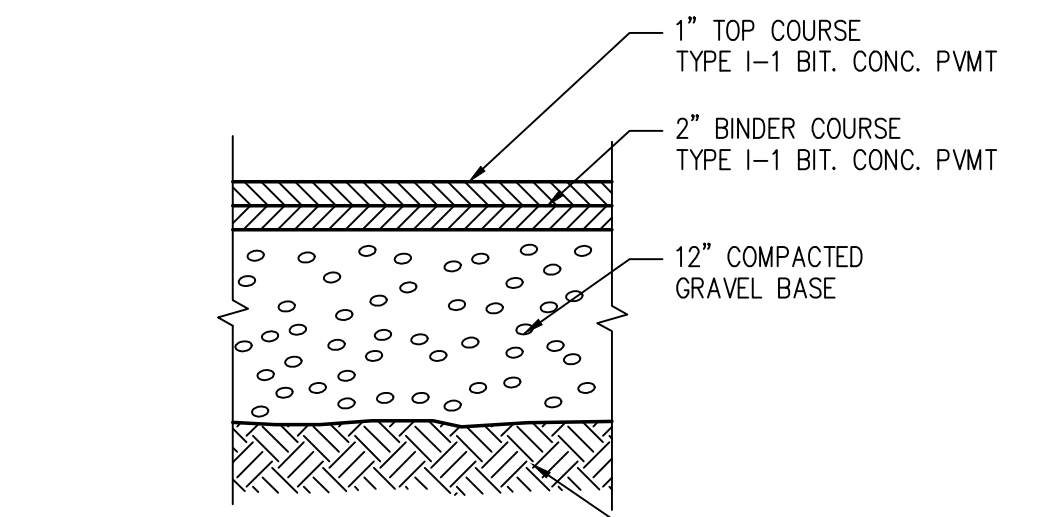
DATE: NOVEMBER 5, 2024

EROSION & SEDIMENTATION CONTROL PLAN

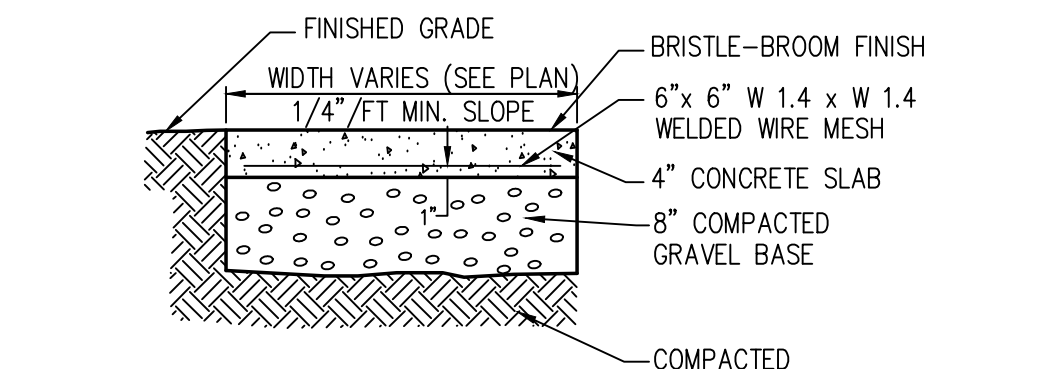
SHEET NO.: SHEET 9 OF 10

PROJECT NO.: G-698

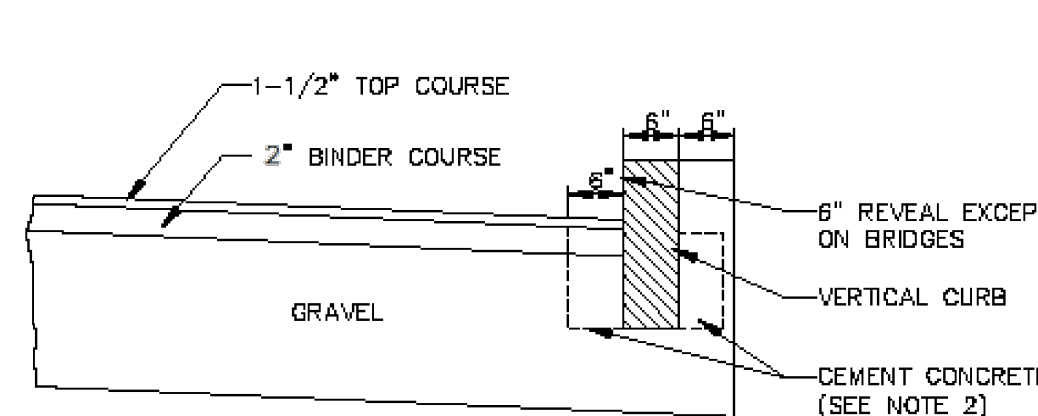




STANDARD BITUMINOUS CONCRETE PAVEMENT
NOT TO SCALE

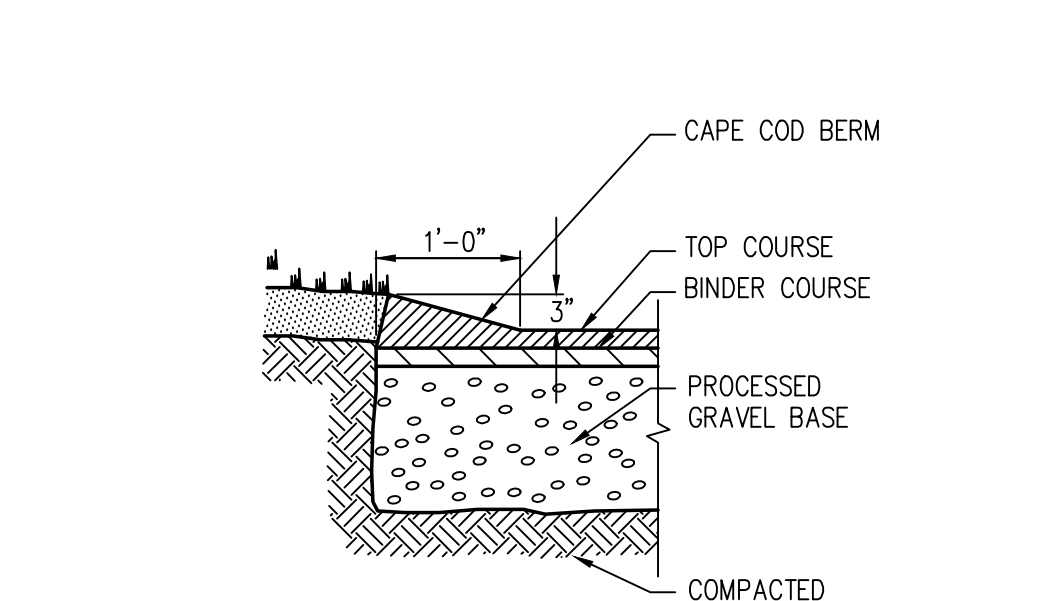


CEMENT CONCRETE WALK
NOT TO SCALE

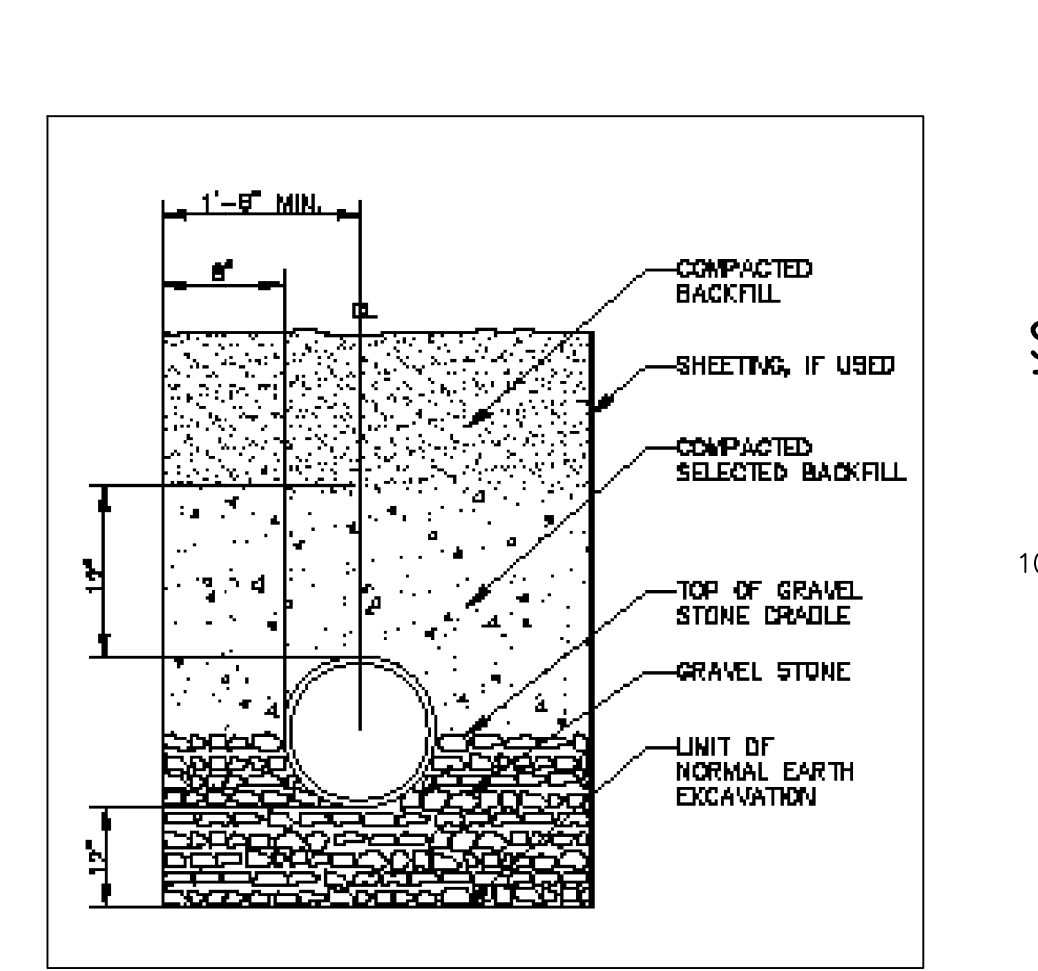


- NOTES:**
- CUT NEAT LINE 6" FROM CURB LINE AND REMOVE BINDER, BASE AND STONE, REPLACE WITH CEMENT CONCRETE.
 - ANY CLASS CEMENT CONCRETE THAT IS ACCEPTABLE TO THE DEPARTMENT UNDER SECTION 44 OF THE 1973 STANDARD SPECIFICATIONS. ALL TEST REQUIREMENTS ARE WAIVED. BITUMINOUS CONCRETE SHALL NOT BE USED AS A SUBSTITUTE.
 - PAYMENT FOR CLASS B CONCRETE WILL BE PAID FOR UNDER ITEM 446.1.

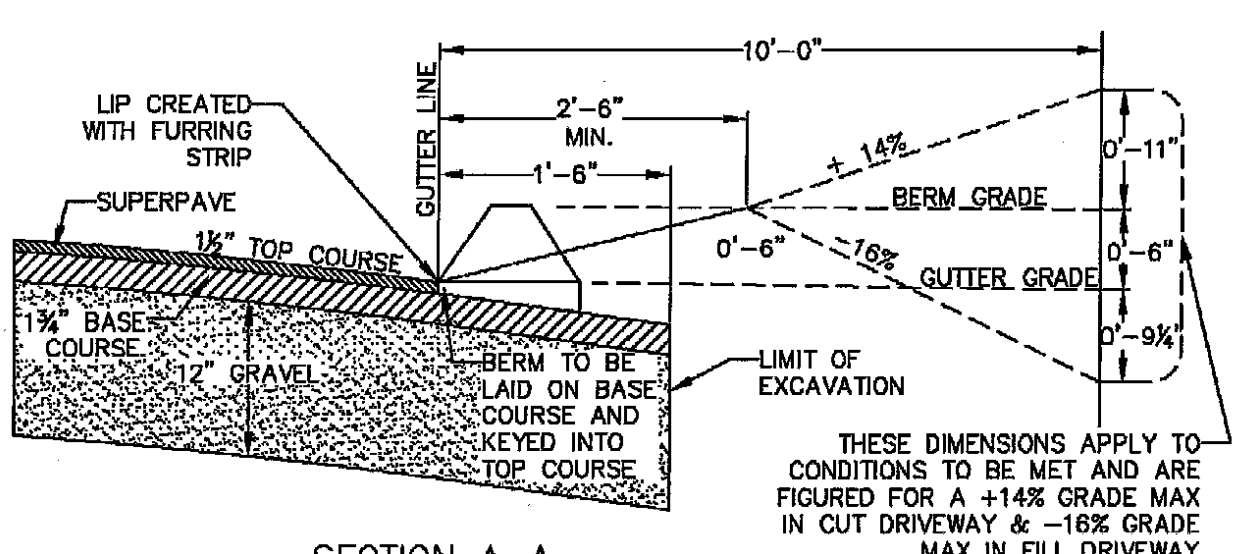
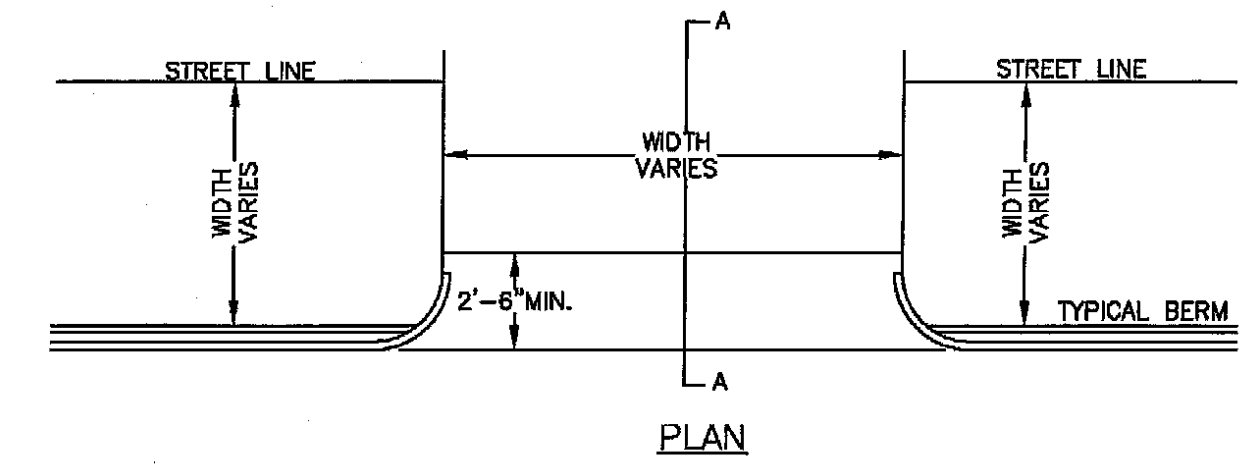
VERTICAL GRANITE CURB DETAIL
NOT TO SCALE



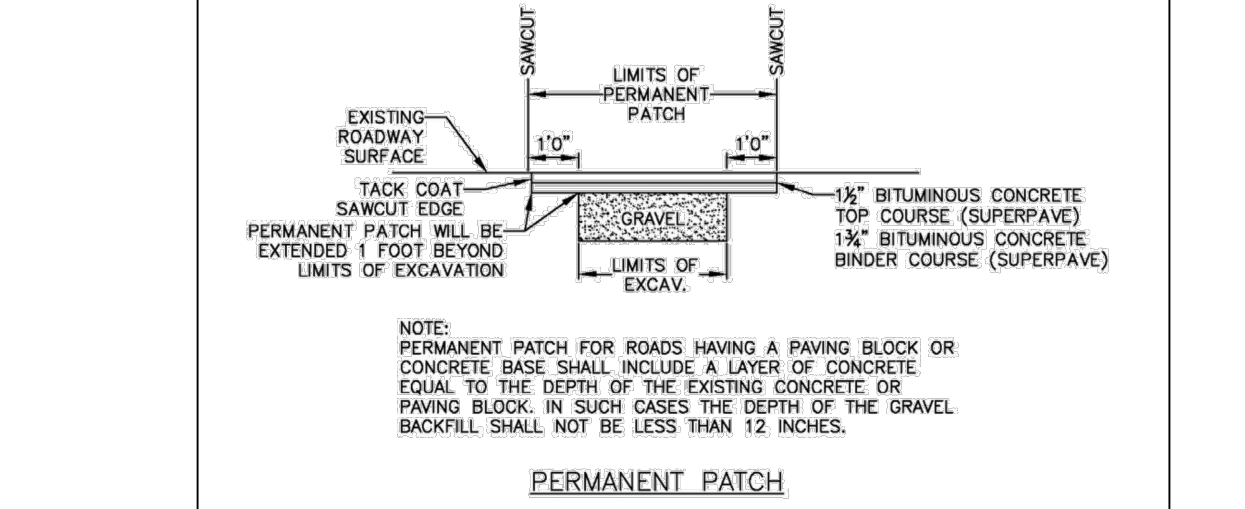
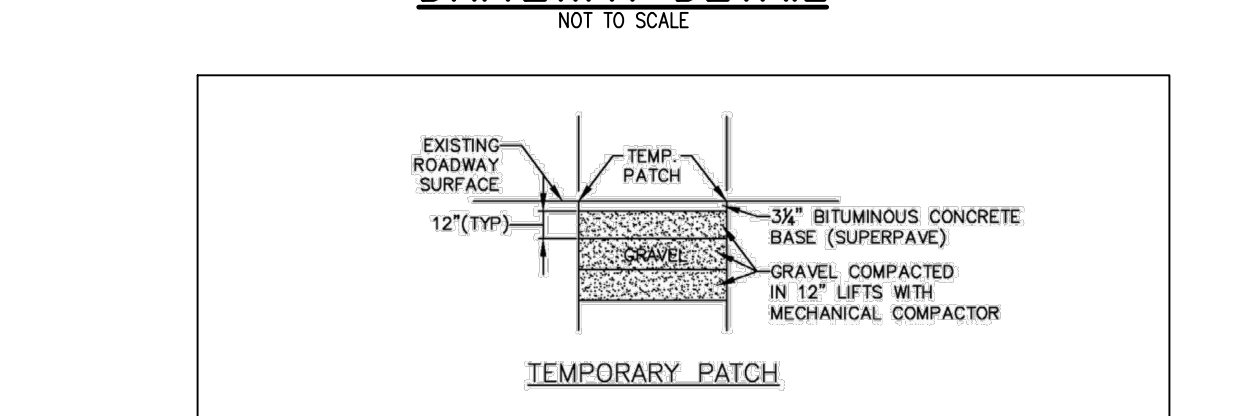
CAPE COD BERM
NOT TO SCALE



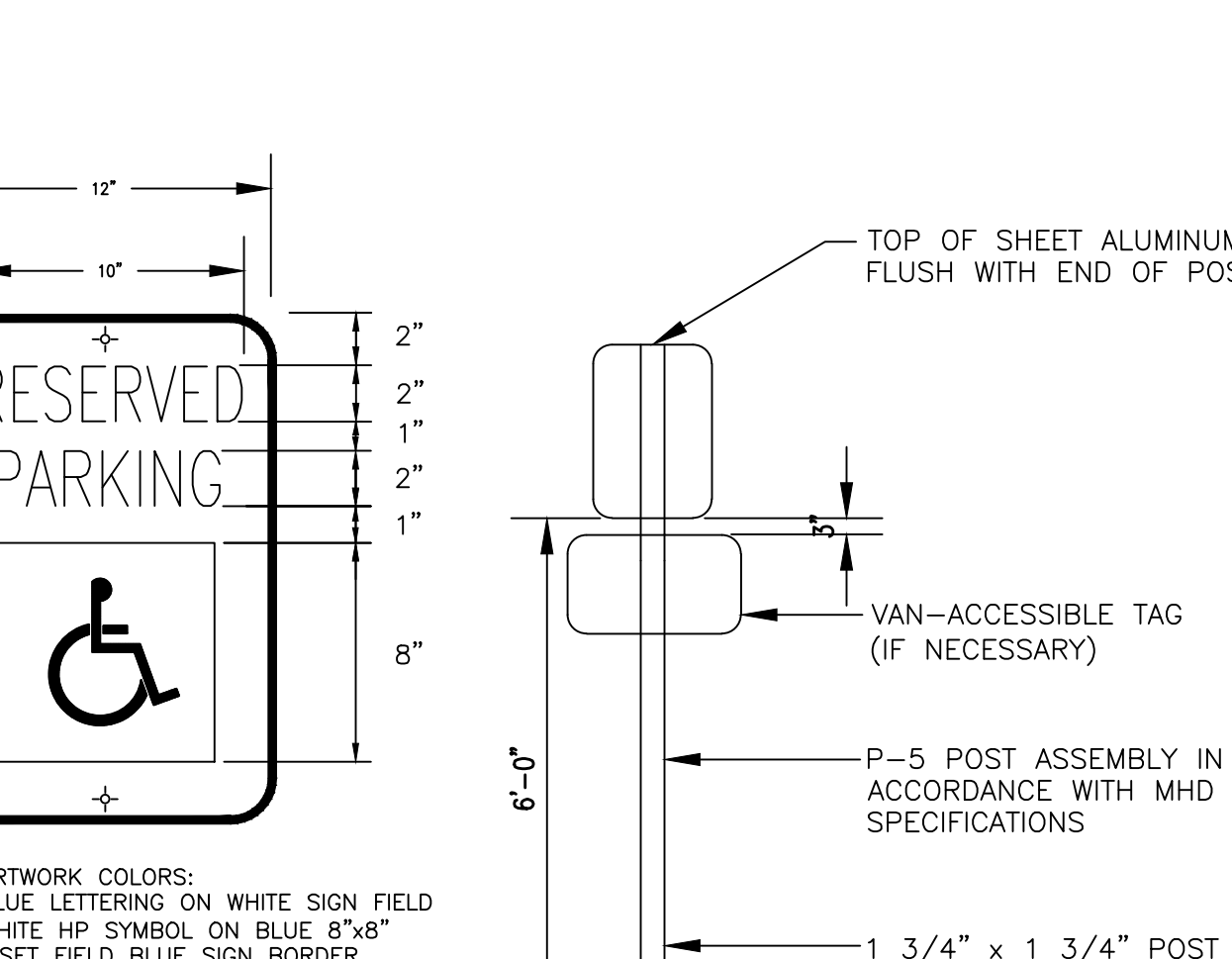
DRAIN TRENCH DETAIL
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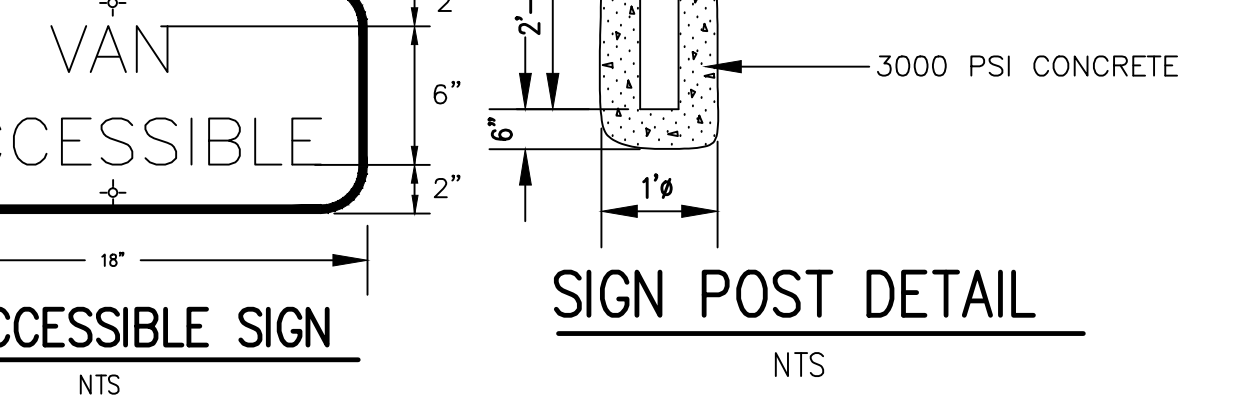
DRIVEWAY DETAIL
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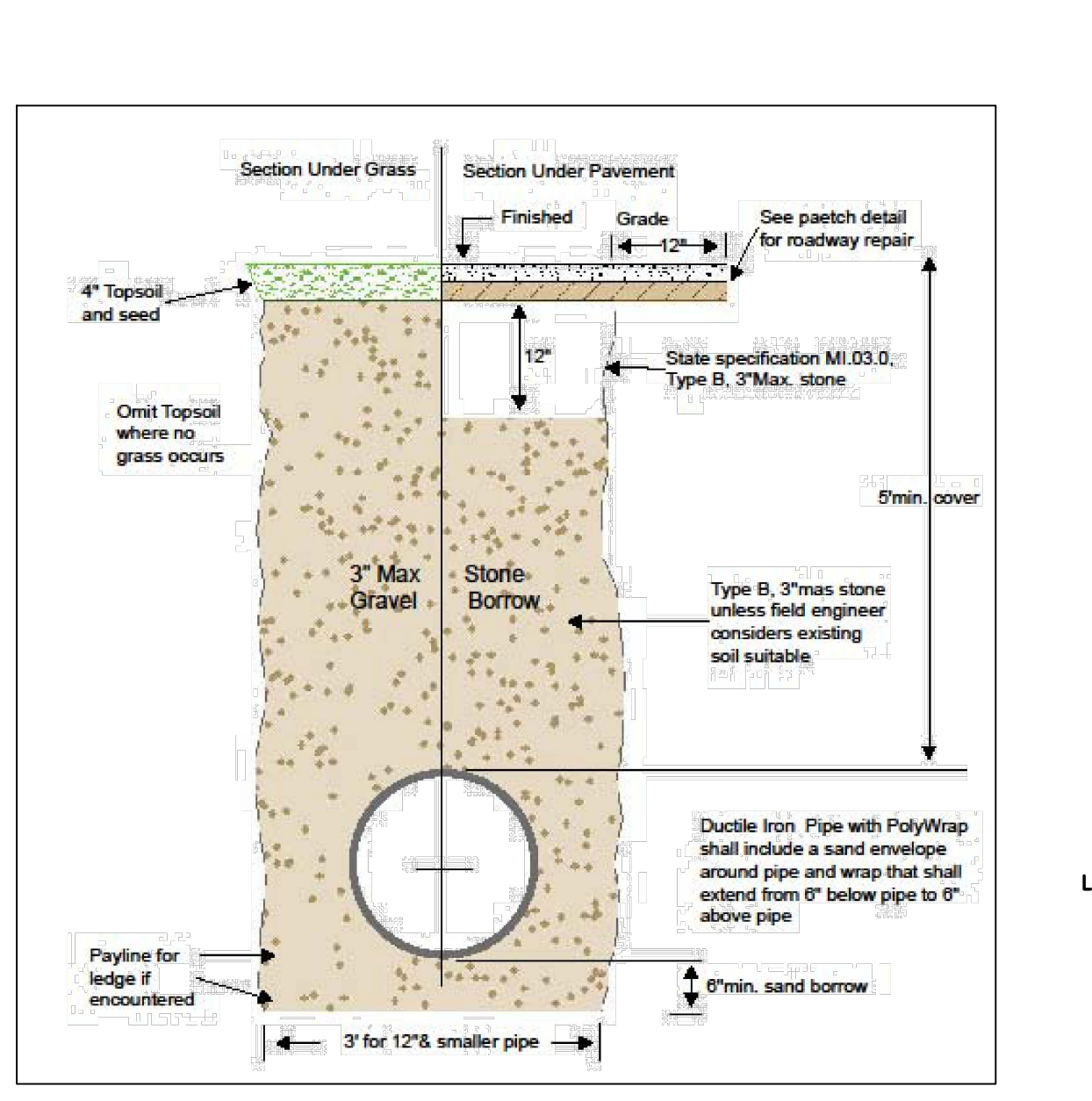
PAVEMENT REPAIR
NOT TO SCALE



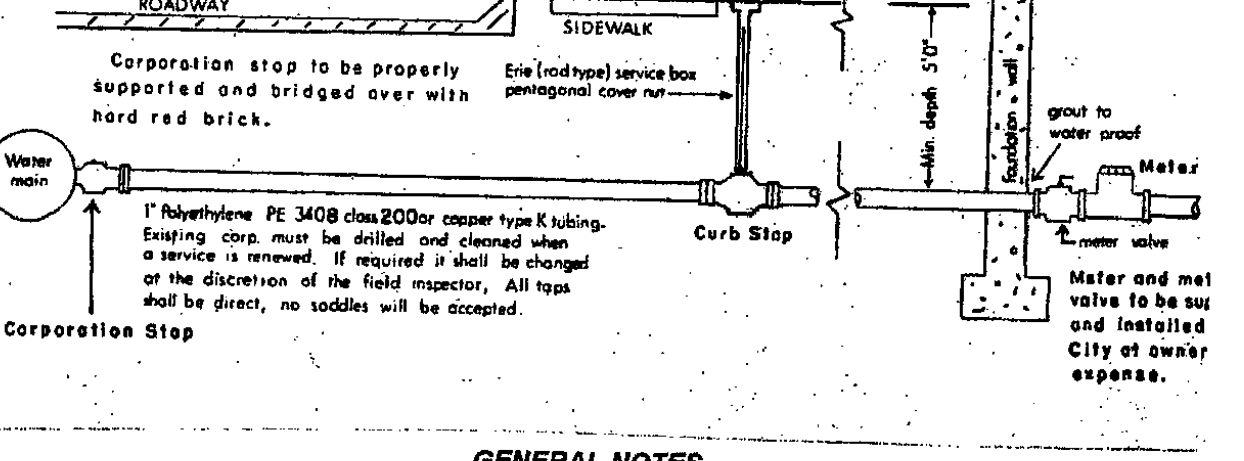
SIGN DETAIL FOR HP PARKING
NTS



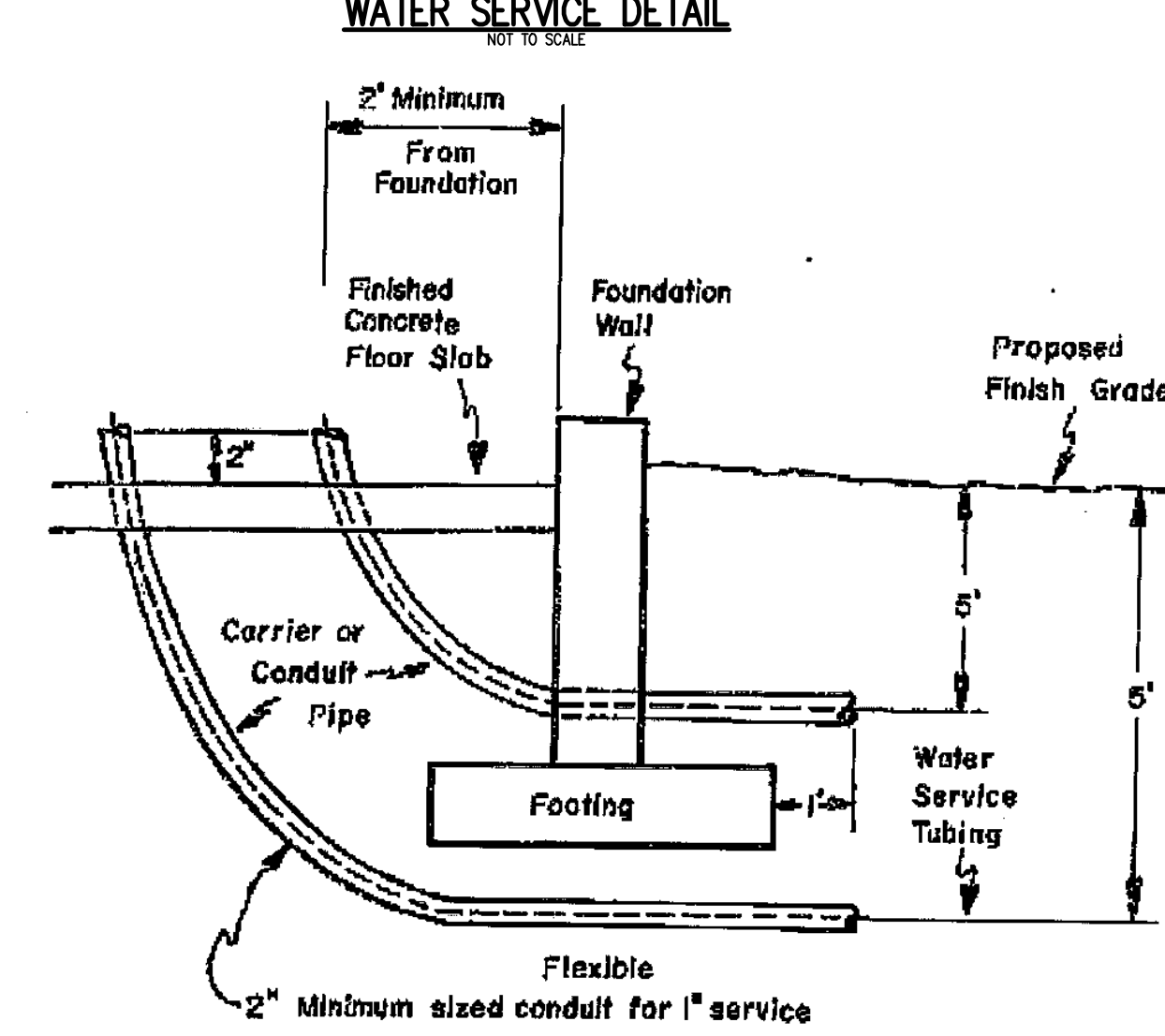
VAN-ACCESSIBLE SIGN
NTS



TYPICAL WATER TRENCH SECTION
NOT TO SCALE

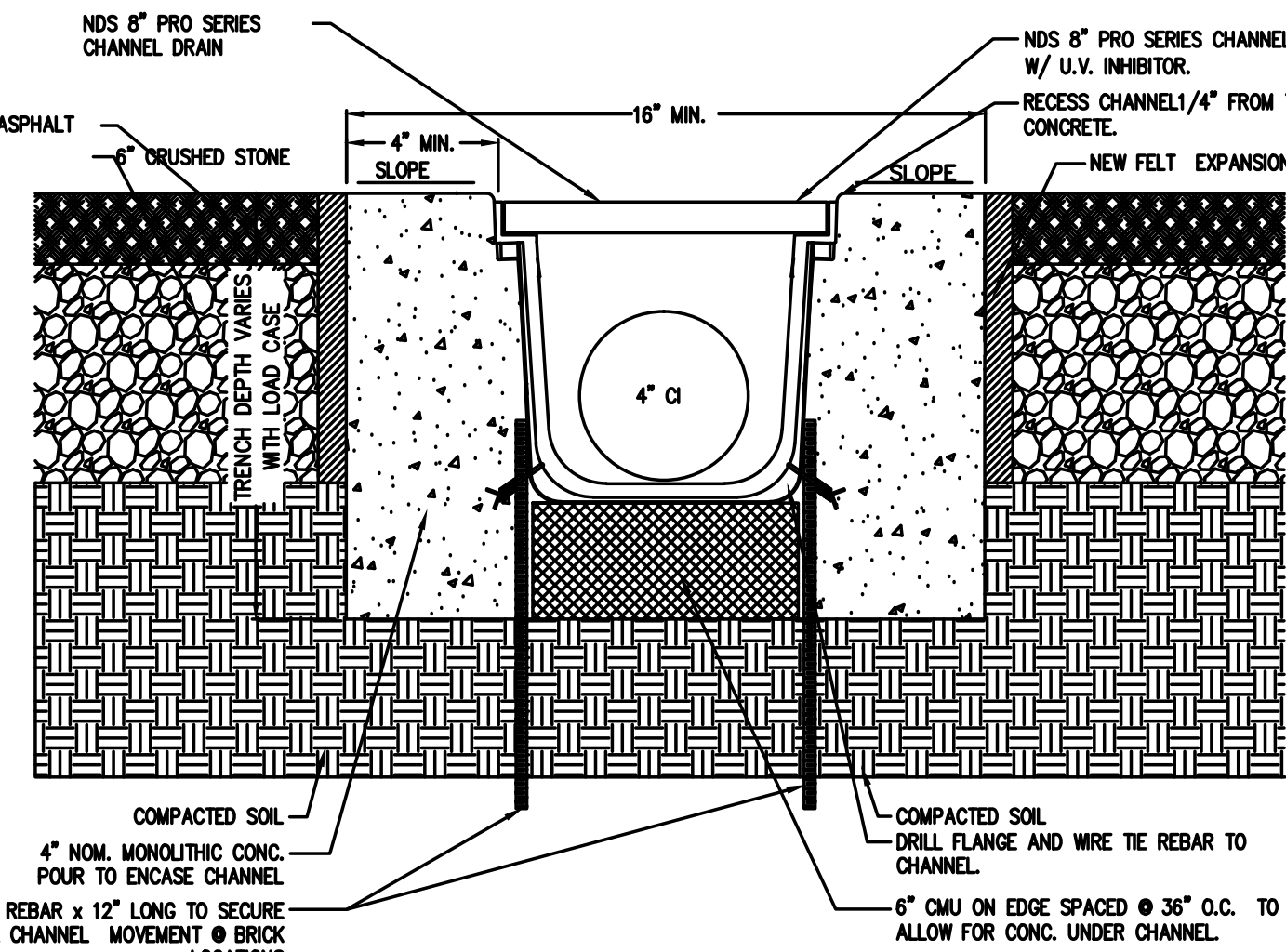


- GENERAL NOTES**
- Fittings and valves shall be manufactured by Ford or approved equal. Corporations: Ford FB1000 c.c. thread by compression. Curb Stops: B44 series full port (comp. x comp.)
 - All compression fittings used on polyethylene tubing shall be installed with solid, tubular stainless steel inset stiffeners.
 - Sand shall be used for backfilling from a point 6 inches below the pipe to a point 12 inches above the pipe. The remaining backfill shall have no stones greater than 3 inches in their largest dimension. Before any excavation is done and any pipe is installed, permits shall be obtained from the Management Service Office at 20 East Worcester St. and the Water Engineering office at 18 East Worcester St. All paving shall conform to City of Worcester D.P.W. standards.
 - Services shall be flushed before activating to avoid meter clogging.
 - Services shall be operated by the City only. Costs incurred in shutting down mains due to negligence of the Contractor as determined by the Field Inspector shall be paid by the Contractor.
 - The Contractor shall inform the owner and the City wire inspector of proper grounding requirements pursuant to M.G.L. Chapter 165, Section 1C.
 - On new services, in accordance with the Mass. Department of Environmental Protection Code 5 and its amendments, the water service shall be a minimum of 10 horizontal feet from the sewer service.
 - The Contractor shall furnish and completely install the service from corporation to the meter valves with new materials.
 - Upon installation of the service box, the Contractor shall operate the stop to the satisfaction of the Field Inspector prior to acceptance of the job.

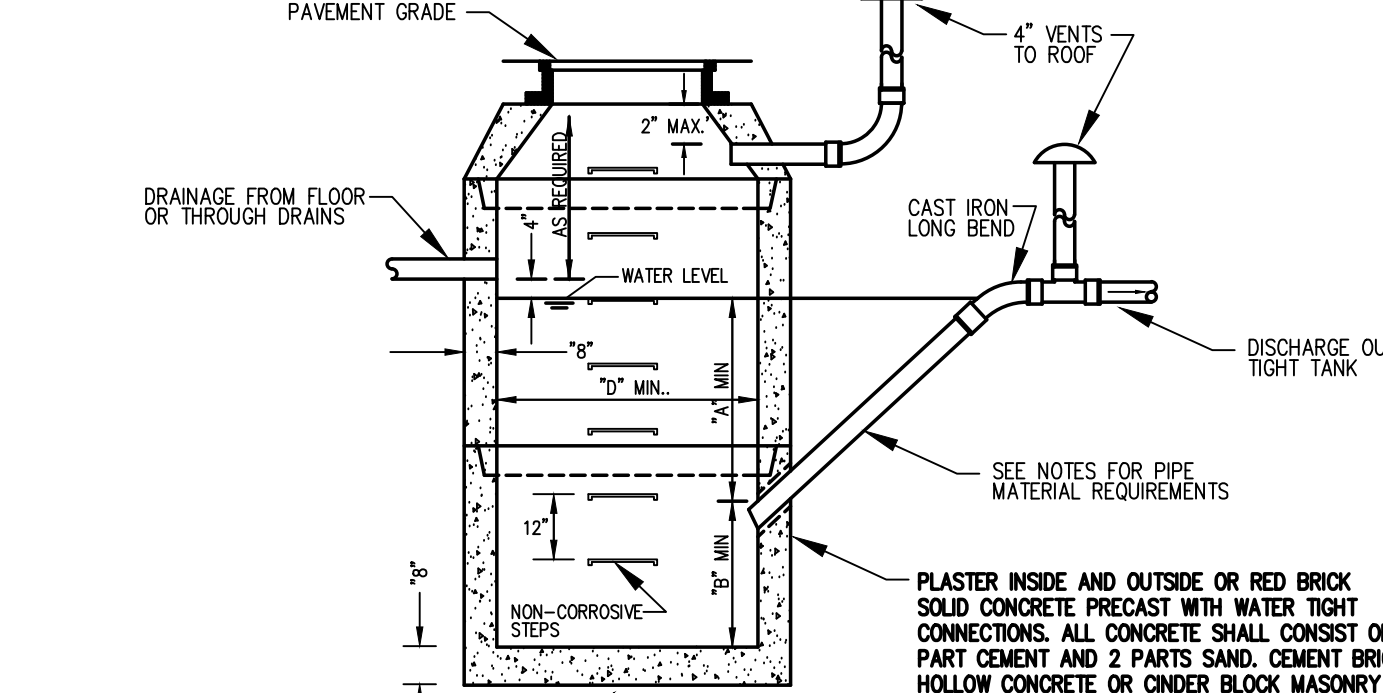


WATER SERVICE ENTRANCE
NOT TO SCALE

- GENERAL NOTES**
- When any water service 1-inch or less in size is to be installed under a concrete floor slab, the pipe shall be installed inside an approved conduit. This conduit shall be used as a sleeve or carrier pipe to receive the proposed water service tubing.
 - The size of the conduit shall be at least twice the diameter of the water service pipe being installed.
 - The conduit shall be installed as shown in drawing W-5D which is part of these specifications.



FLOOR DRAIN CROSS-SECTION
NOT TO SCALE



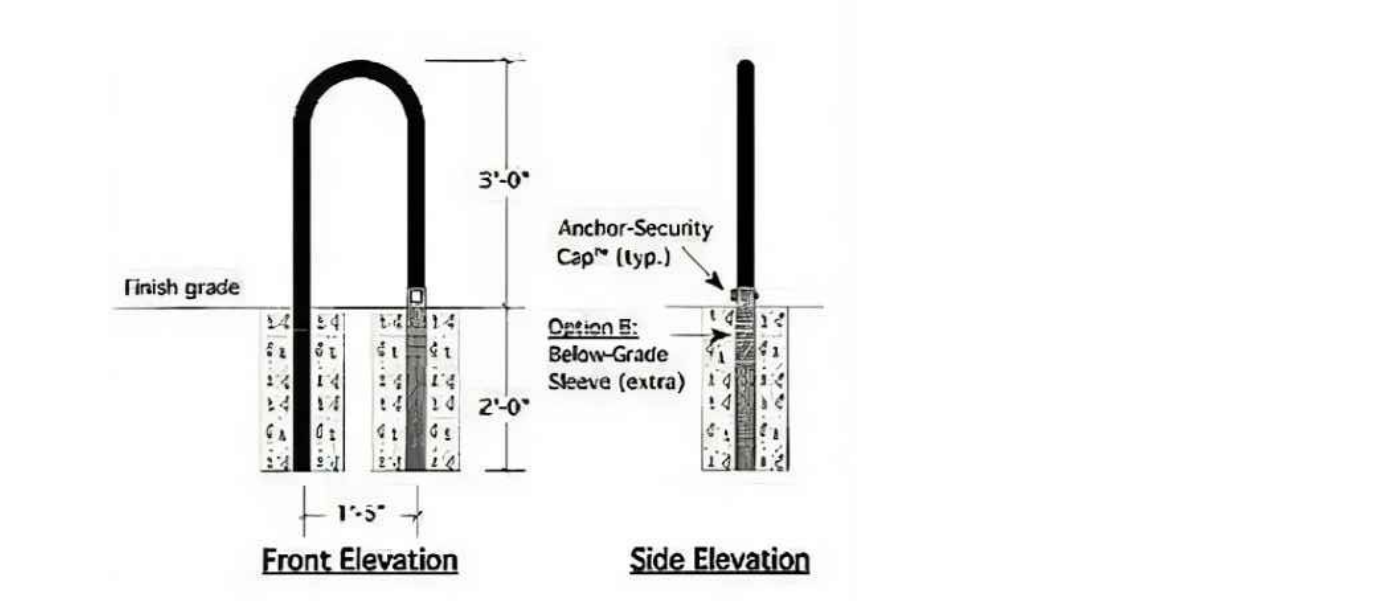
GAS OIL & WATER SEPARATOR
NOT TO SCALE

PIPE MATERIAL NOTES:

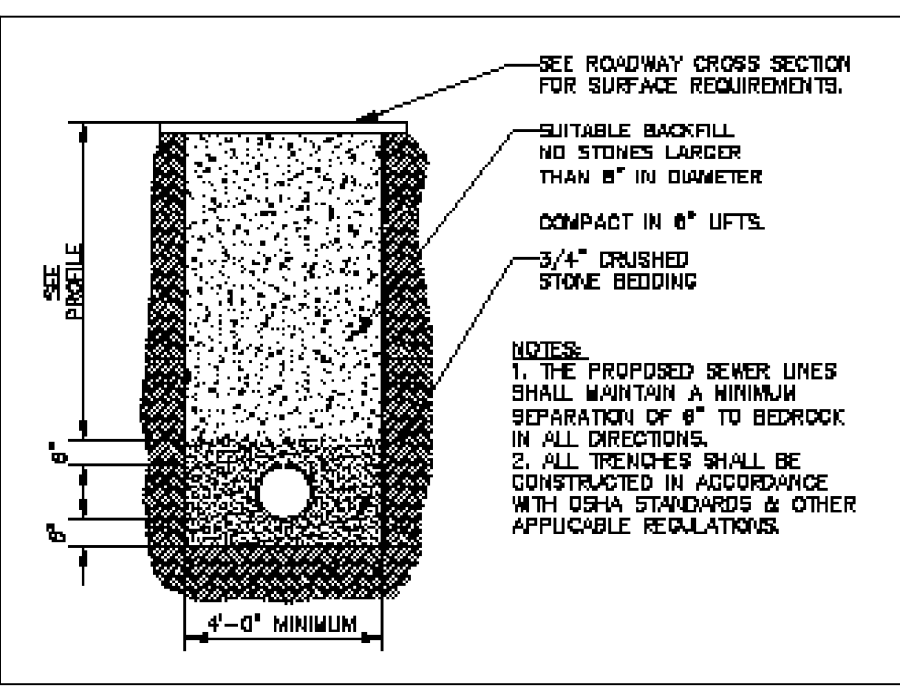
- NO-HUB CAST IRON WITH PRODUCT-APPROVED STAINLESS STEEL GASKETS.
- SERVICE WEIGHT CAST IRON WITH PRODUCT-APPROVED RESILIENT GASKETS OR LEAD AND OAKUM JOINTS.
- EXTRA HEAVY CAST IRON WITH PRODUCT APPROVED RESILIENT GASKETS OR LEAD AND OAKUM JOINTS.

GENERAL CONSTRUCTION NOTES:

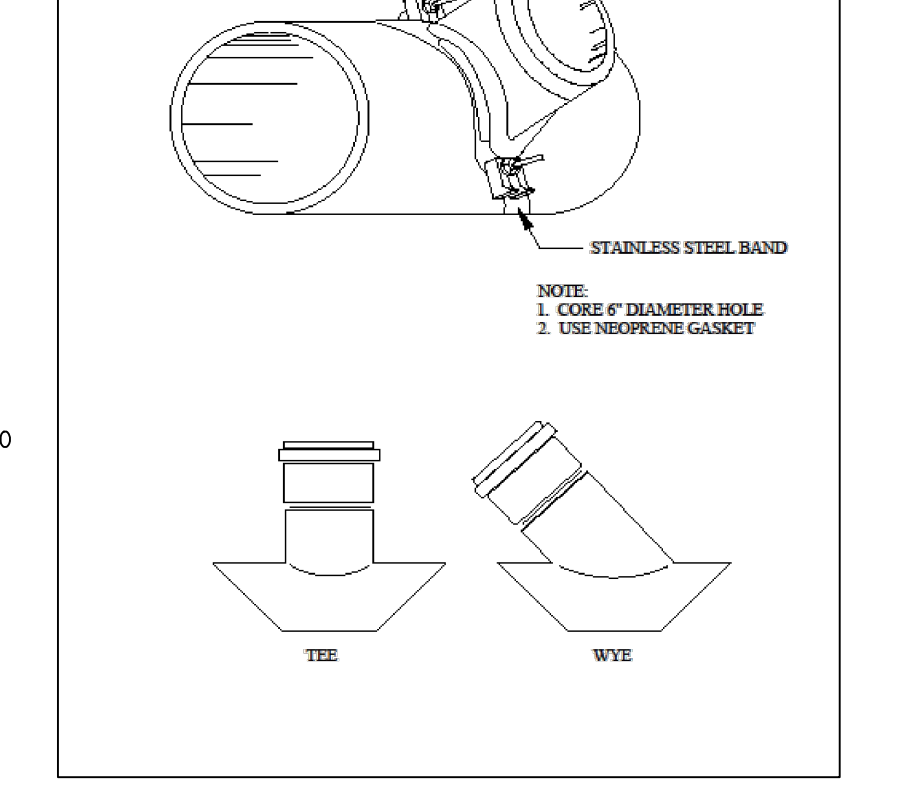
- THE SEPARATOR IS TO BE LOCATED OUTSIDE OF A BUILDING WHERE POSSIBLE AND THE COVER TO INCORPORATE A CENTER HOLE. A SEALED TIGHT COVER IS TO BE USED IF THE SEPARATOR IS LOCATED INSIDE OF A BUILDING. THE COVER SHALL BE NO LESS THAN A 24" DIAMETER. THE SEPARATOR SHALL BE LOCATED AND CONSTRUCTED TO PREVENT SURFACE OR SUB-SURFACE WATER FROM ENTERING. THE INLET PIPE SHALL BE NO LESS THAN FOUR INCHES ABOVE THE WATER LINE LEVEL. WHEN THE SEPARATOR IS SUBJECT TO FREEZING IT SHALL BE SET A MINIMUM OF THREE FEET BELOW GRADE. THE NON-CORROSIVE STEPS SHALL BE PLACED AT 18" APART. THE CHAMBER VENT AND OUTLET VENT SHALL RETURN TO THE INSIDE OF THE BUILDING AND EXTEND THROUGH THE ROOF.
- IN OPEN PARKING GARAGES OR OPEN PARKING AREAS ONLY THE INLET PIPE MAY EXTEND BELOW THE WATER LINE A MAXIMUM DISTANCE OF 6".
- CIRCULAR BASINS ARE RECOMMENDED.



BICYCLE RACK (OR EQUAL)
NOT TO SCALE



SEWER TRENCH DETAIL
NOT TO SCALE



SEWER TEE & WYE CONNECTION
NOT TO SCALE

SIZING TABLE

INLET	D	A	B
4"	3'-6"	3'-0"	2'-6"
5"	3'-6"	5'-0"	4'-0"
	3'-6"x3'-6"	4'-0"	3'-0"
	4'-0"	3'-6"	3'-0"
	3'-6"x3'-6"	3'-0"	2'-6"
	4'-6"	3'-0"	2'-6"
6"	4'-0"	5'-0"	4'-6"
	4'-0"x4'-0"	4'-0"	3'-6"
	4'-6"	4'-0"	3'-6"
	4'-6"x4'-6"	3'-0"	2'-6"
	5'-0"	3'-6"	3'-0"
	5'-0"x5'-0"	3'-0"	2'-6"
8"	5'-0"	6'-0"	5'-0"
	5'-6"x5'-6"	4'-6"	4'-0"
	6'-0"	4'-0"	3'-6"
	6'-0"x6'-0"	3'-0"	2'-6"
	6'-6"	3'-6"	3'-0"
	6'-6"x6'-6"	3'-0"	2'-6"

REV. NO.	DATE	REVISION

DEFINITIVE SITE PLAN
FOR
216 & 221 CHANDLER STREET
WORCESTER, MASSACHUSETTS 01610

PREPARED FOR:
POLAR VIEWS, LLC
89 WEST MAIN STREET UNIT 101
NORTHBOROUGH, MASSACHUSETTS 01532

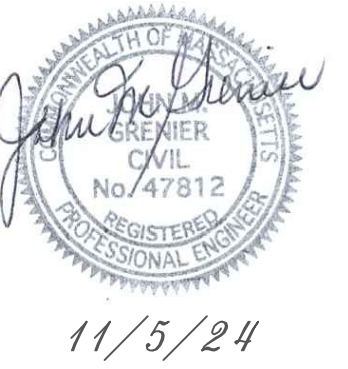
PROPERTY OWNER:
DANIEL YARNIE
89 WEST MAIN STREET UNIT 101
NORTHBOROUGH, MASSACHUSETTS 01532

PREPARED BY:
J.M. GRENIER ASSOCIATES INC.
118 TURNPIKE ROAD SUITE 200
SOUTHBOROUGH, MASSACHUSETTS 01772

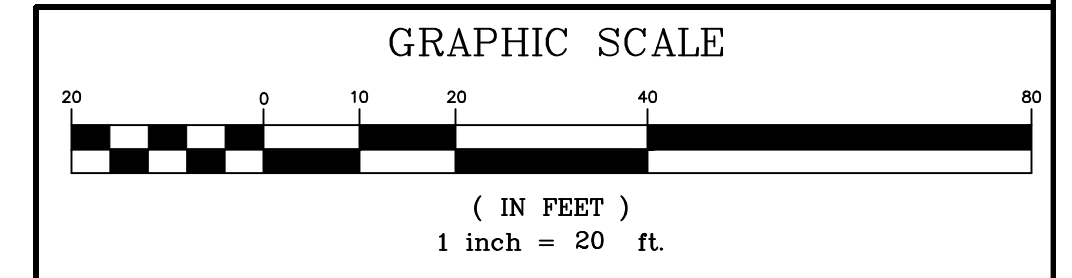
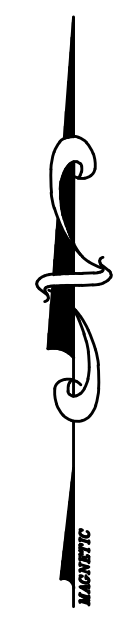
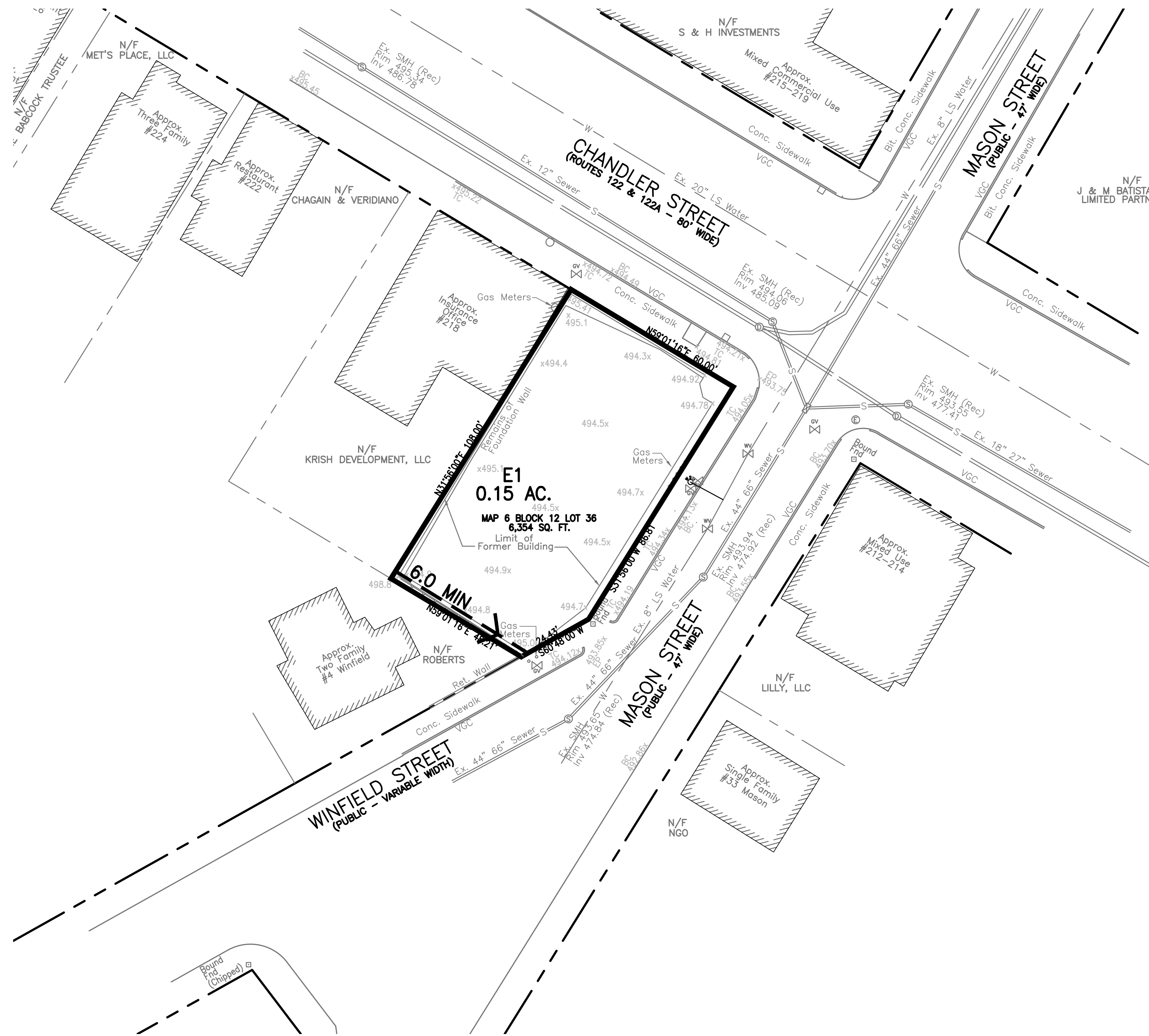
TELE NO.: (508) 845-2500 jmgrenier@townisp.com
SCALE: AS SHOWN DATE: NOVEMBER 5, 2024

DETAIL PLAN

SHEET NO.: SHEET 10 OF 10 PROJECT NO.: G-698



11/5/24



REV. NO.	DATE	REVISION

TITLE:
DEFINITIVE SITE PLAN
FOR
216 & 221 CHANDLER STREET
WORCESTER, MASSACHUSETTS 01610

PREPARED FOR:
POLAR VIEWS, LLC
89 WEST MAIN STREET UNIT 101
NORTHBOROUGH, MASSACHUSETTS 01532

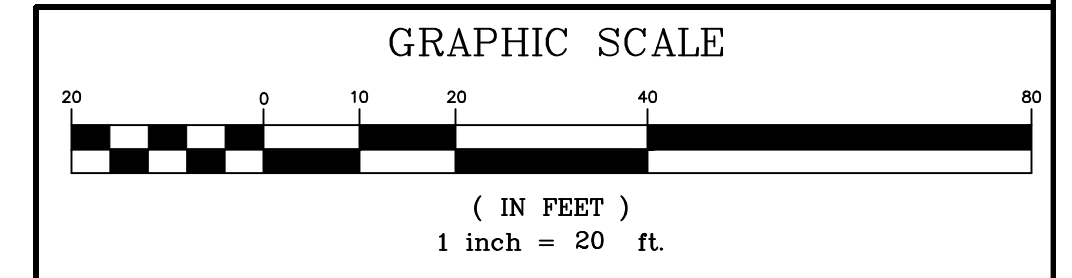
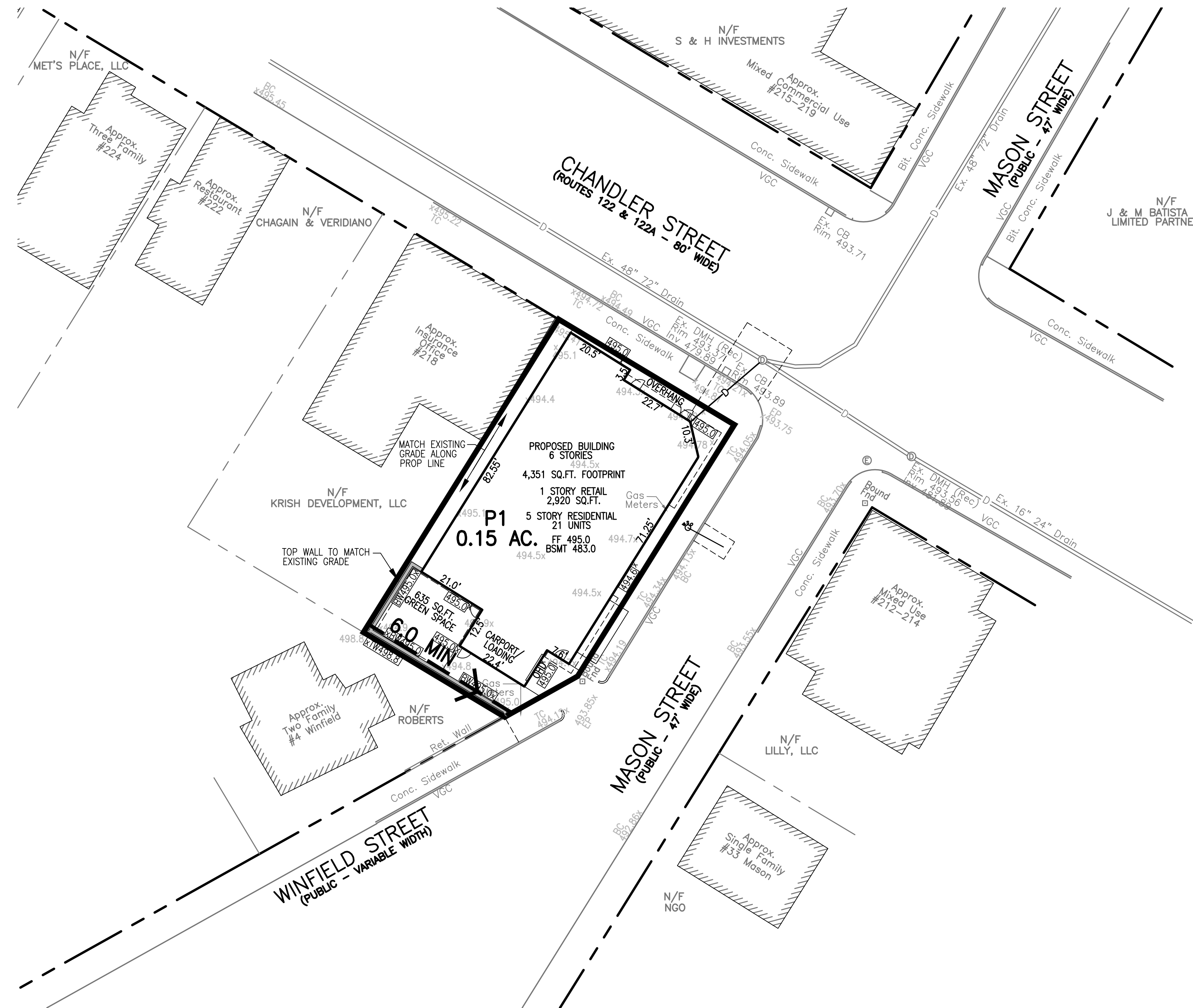
PROPERTY OWNER:
DANIEL YARNIE
89 WEST MAIN STREET UNIT 101
NORTHBOROUGH, MASSACHUSETTS 01532

PREPARED BY:
J.M. GRENIER ASSOCIATES INC.
118 TURNPIKE ROAD SUITE 200
SOUTHBOROUGH, MASSACHUSETTS 01772
TELE NO.: (508) 845-2500 jmgrenier@townisp.com

SCALE: 1" = 20' **DATE:** NOVEMBER 5, 2024

PRE-DEVELOPMENT DRAINAGE AREAS

SHEET NO.: SHEET 1 OF 2 **PROJECT NO.:** G-698



REV. NO.	DATE	REVISION

TITLE:
DEFINITIVE SITE PLAN
FOR
216 & 221 CHANDLER STREET
WORCESTER, MASSACHUSETTS 01610

PREPARED FOR:
POLAR VIEWS, LLC
89 WEST MAIN STREET UNIT 101
NORTHBOROUGH, MASSACHUSETTS 01532

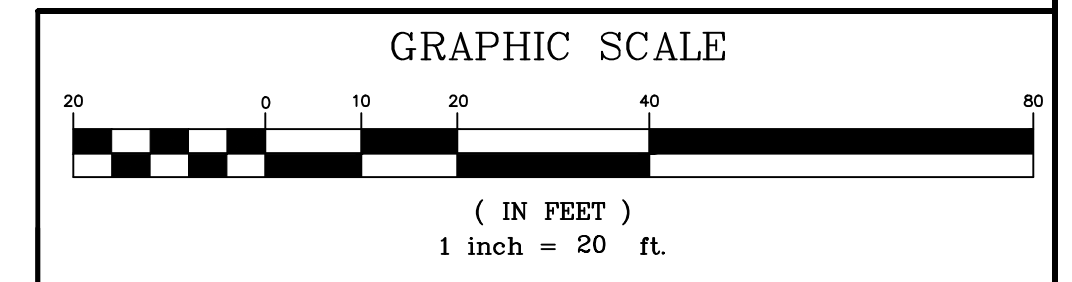
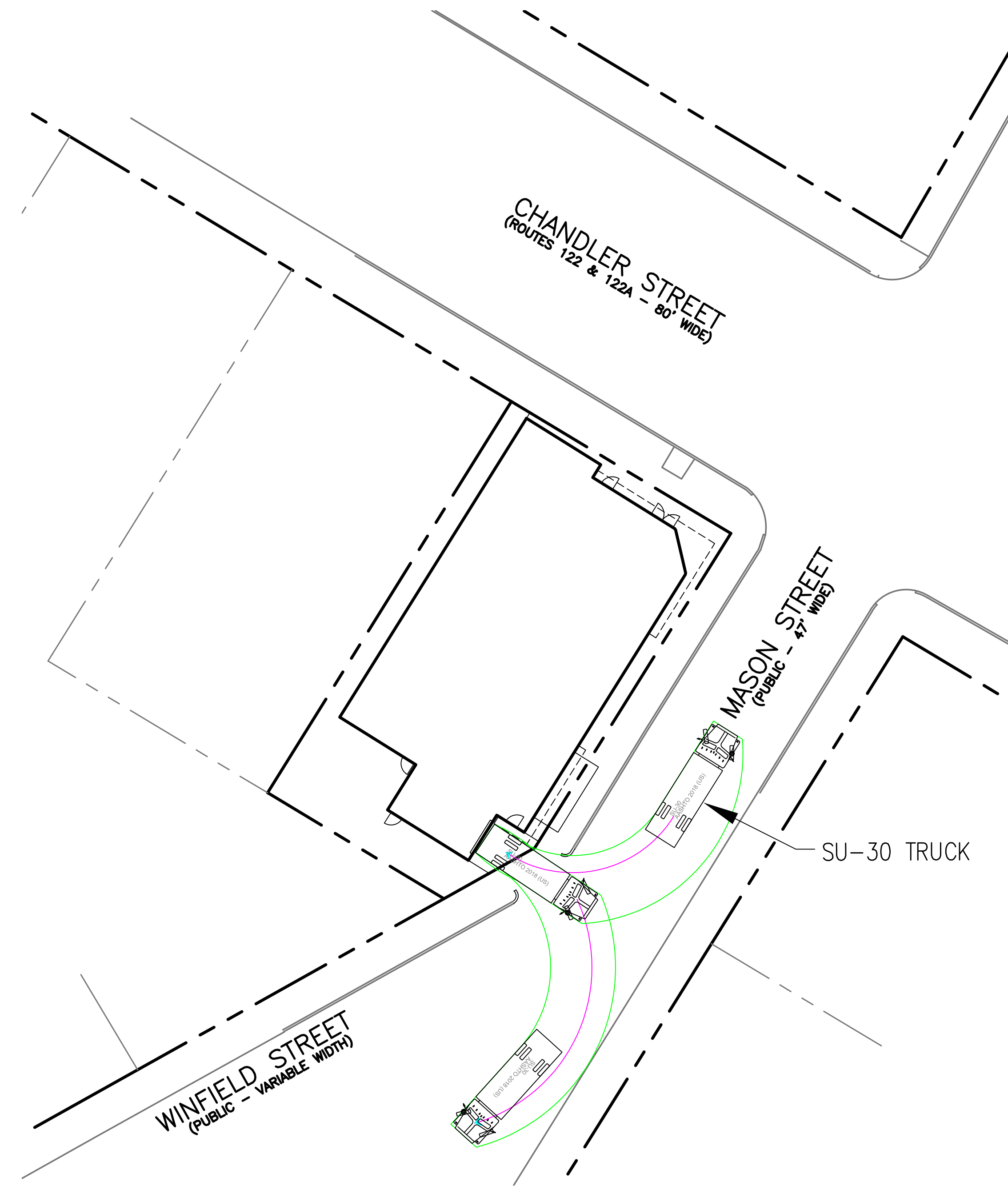
PROPERTY OWNER:
DANIEL YARNIE
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NORTHBOROUGH, MASSACHUSETTS 01532

PREPARED BY:
J.M. GRENIER ASSOCIATES INC.
118 TURNPIKE ROAD SUITE 200
SOUTHBOROUGH, MASSACHUSETTS 01772
TELE NO.: (508) 845-2500 jmgrenier@townisp.com

SCALE: 1" = 20' **DATE:** NOVEMBER 5, 2024

POST-DEVELOPMENT DRAINAGE AREAS

SHEET NO.: SHEET 2 OF 2 **PROJECT NO.:** G-698



REV. NO.	DATE	REVISION

TITLE:
DEFINITIVE SITE PLAN
FOR
216 & 221 CHANDLER STREET
WORCESTER, MASSACHUSETTS 01610

PREPARED FOR:
POLAR VIEWS, LLC
89 WEST MAIN STREET UNIT 101
NORTHBOROUGH, MASSACHUSETTS 01532

PROPERTY OWNER:
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89 WEST MAIN STREET UNIT 101
NORTHBOROUGH, MASSACHUSETTS 01532

PREPARED BY:
J.M. GRENIER ASSOCIATES INC.
118 TURNPIKE ROAD SUITE 200
SOUTHBOROUGH, MASSACHUSETTS 01772

TELE NO.: (508) 845-2500 jmgrenier@townisp.com

SCALE: 1" = 20'	DATE: NOVEMBER 5, 2024
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BOX TRUCK TURNING PLAN

SHEET NO.: SHEET 1 OF 1	PROJECT NO.: G-698
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